

# Exhibit C

## Responses BAFO 24-08-C Corporate Drive Segments 2 & 3

Success: All data is valid!

Status	#	Item	Item Description	Quantity Required	Unit of Measure	Numeric	Total Cost
						Unit Price	

### Corporate Drive Segments @ & 3

Success: All values provided	#1-1	Mobilization	Start-up (Bonds, Insurance, Move-In, Sanitary Facilities) not to exceed 5%	1	Lump Sum	\$ 2,018,313.00	\$ 2,018,313.00
Success: All values provided	#1-2	Temporary Traffic Control	Includes Barriers, Barricades, and Detour Signs	1	Lump Sum	\$ 86,000.00	\$ 86,000.00
Success: All values provided	#1-3	Prepare Right of Way	.	84	Stations	\$ 25,000.00	\$ 2,100,000.00
Success: All values provided	#1-4	Sawcut and Remove Concrete Flatwork	Includes sawcutting, removal and disposal	4035	Square Feet	\$ 2.65	\$ 10,692.75
Success: All values provided	#1-5	Sawcut and Remove Curb and Gutter	Includes sawcutting, removal and disposal	529	Linear Feet	\$ 7.50	\$ 3,967.50
Success: All values provided	#1-6	Remove Gravel Pavement	.	3494	Square Feet	\$ 1.15	\$ 4,018.10
Success: All values provided	#1-7	Remove Fences (All Types)	.	325	Linear Feet	\$ 5.20	\$ 1,690.00
Success: All values provided	#1-8	Remove Headwall (for 4 feet by 3 feet Box Culvert)	.	2	Each	\$ 2,220.00	\$ 4,440.00
Success: All values provided	#1-9	Unclassified Excavation	.	22973	Cubic Yard	\$ 12.18	\$ 279,811.14
Success: All values provided	#1-10	Borrow from Mitigation Site	.	68556	Cubic Yard	\$ 15.00	\$ 1,028,340.00
Success: All values provided	#1-11	Install Barbed Wire Fence	.	59	Linear Feet	\$ 20.00	\$ 1,180.00
Success: All values provided	#1-12	Clearing and Grubbing	.	30	Acres	\$ 3,500.00	\$ 105,000.00
Success: All values provided	#1-13	Heavy Tree Clearing and Grubbing	.	11.51	Acres	\$ 9,072.00	\$ 104,418.72
Success: All values provided	#1-14	Erosion Control (SWPPP) and Removal	.	1	Lump Sum	\$ 350,000.00	\$ 350,000.00
Success: All values provided	#1-15	4-Inch Topsoil and Hydromulch Seeding	.	54300	Square Yard	\$ 2.12	\$ 115,116.00
Success: All values provided	#1-16	4-Inch Topsoil and Seeding with Soil Retention Blanket	.	59613	Square Yard	\$ 3.14	\$ 187,184.82
Success: All values provided	#1-17	Concrete Riprap Type RR8	.	1581	Square Yard	\$ 132.00	\$ 208,692.00
Success: All values provided	#1-18	6-Inch Lime Stabilized Subgrade	.	38284	Square Yard	\$ 8.60	\$ 329,242.40
Success: All values provided	#1-19	Furnish Lime (36 pounds per square yard)	.	688	Ton	\$ 301.00	\$ 207,088.00
Success: All values provided	#1-20	8-Inch Flex Base	.	3807	Square Yard	\$ 24.00	\$ 91,368.00
Success: All values provided	#1-21	10-Inch Reinforced Concrete Pavement Including 6-Inch Curb	.	35753	Square Yard	\$ 115.00	\$ 4,111,595.00
Success: All values provided	#1-22	Concrete Header	.	100	Linear Feet	\$ 82.00	\$ 8,200.00
Success: All values provided	#1-23	Anchorage Joint	.	968	Linear Feet	\$ 18.00	\$ 17,424.00
Success: All values provided	#1-24	Sawtooth Curb and Gutter	.	90	Linear Feet	\$ 80.00	\$ 7,200.00
Success: All values provided	#1-25	Concrete Curb and Gutter	.	133	Linear Feet	\$ 64.00	\$ 8,512.00
Success: All values provided	#1-26	5-Inch Thick Concrete Sidewalk	.	102255	Square Feet	\$ 10.00	\$ 1,022,550.00

Success: All values provided	#1-27	Stamped Concrete Median Pavement	.	1519	Square Feet	\$ 26.00	\$ 39,494.00
Success: All values provided	#1-28	6-Inch Gravel Driveway	.	307	Square Yard	\$ 32.00	\$ 9,824.00
Success: All values provided	#1-29	Barrier-Free Sidewalk Ramp -12-Feet Modified Type 3 (5-Inch Thick Concrete ADA Ramp)	.	3	Each	\$ 6,100.00	\$ 18,300.00
Success: All values provided	#1-30	Barrier-Free Sidewalk Ramp -5-Feet Modified Type 7 (5-Inch Thick Concrete ADA Ramp)	.	1	Each	\$ 3,900.00	\$ 3,900.00
Success: All values provided	#1-31	Barrier-Free Sidewalk Ramp -6-Feet Modified Type 7 (5-Inch Thick Concrete ADA Ramp)	.	2	Each	\$ 4,300.00	\$ 8,600.00
Success: All values provided	#1-32	Barrier-Free Sidewalk Ramp -12-Feet Modified Type 7 (5-Inch Thick Concrete ADA Ramp)	.	1	Each	\$ 6,000.00	\$ 6,000.00
Success: All values provided	#1-33	Barrier-Free Sidewalk Ramp -12-Feet Modified Type 21 (5-Inch Thick Concrete ADA Ramp)	.	1	Each	\$ 5,900.00	\$ 5,900.00
Success: All values provided	#1-34	Barrier-Free Sidewalk Ramp -5-Feet Modified Type 21 (5-Inch Thick Concrete ADA Ramp)	.	1	Each	\$ 4,300.00	\$ 4,300.00
Success: All values provided	#1-35	MSE Retaining Wall	.	4004	Square Feet	\$ 140.00	\$ 560,560.00
Success: All values provided	#1-36	Anti-Graffiti Coating (Permanent-TY III)	.	4004	Square Feet	\$ 1.20	\$ 4,804.80
Success: All values provided	#1-37	Type C402 (HPC) Traffic Rail	.	628	Linear Feet	\$ 290.00	\$ 182,120.00
Success: All values provided	#1-38	Traffic Rail Transition	.	160	Linear Feet	\$ 223.00	\$ 35,680.00
Success: All values provided	#1-39	Pedestrian Rail with Coping	.	20	Linear Feet	\$ 741.00	\$ 14,820.00
Success: All values provided	#1-40	Pedestrian Rail with Coping Transition	.	10	Linear Feet	\$ 850.00	\$ 8,500.00
Success: All values provided	#1-41	Drill Shaft (24-Inch)	.	657	Linear Feet	\$ 124.00	\$ 81,468.00
Success: All values provided	#1-42	Drill Shaft (36-Inch)	.	13526	Linear Feet	\$ 248.00	\$ 3,354,448.00
Success: All values provided	#1-43	Drill Shaft (42-Inch)	.	1088	Linear Feet	\$ 362.00	\$ 393,856.00
Success: All values provided	#1-44	Class C Concrete (ABUT) (HPC)	.	211	Cubic Yard	\$ 1,400.00	\$ 295,400.00
Success: All values provided	#1-45	Class C Concrete (CAP) (HPC)	.	654.4	Cubic Yard	\$ 1,790.00	\$ 1,171,376.00
Success: All values provided	#1-46	Class C Concrete (COL) (HPC)	.	381.5	Cubic Yard	\$ 2,158.00	\$ 823,277.00
Success: All values provided	#1-47	Class S Concrete (BAS) (HPC)	.	354.2	Cubic Yard	\$ 795.00	\$ 281,589.00
Success: All values provided	#1-48	Class S Concrete (Median) (HPC)	.	324.3	Cubic Yard	\$ 682.00	\$ 221,172.60
Success: All values provided	#1-49	Reinforced Concrete Slab Class S (HPC)	.	174153	Square Feet	\$ 27.00	\$ 4,702,131.00
Success: All values provided	#1-50	Prestress Concrete Girder (Tx46)	.	19620.28	Linear Feet	\$ 258.00	\$ 5,062,032.24
Success: All values provided	#1-51	Prestress Concrete Girder (Tx54)	.	1894.34	Linear Feet	\$ 306.00	\$ 579,668.04
Success: All values provided	#1-52	Structural Steel (Misc. Non-Bridge) (BS-EJCP) (TXDOT Item 442)	.	3040	Pounds	\$ 26.00	\$ 79,040.00
Success: All values provided	#1-53	C402 Rail (HPC) (Bridge)	.	4378	Linear Feet	\$ 315.00	\$ 1,379,070.00
Success: All values provided	#1-54	Rail (Custom Pedestrian Rail)	.	4380	Linear Feet	\$ 400.00	\$ 1,752,000.00
Success: All values provided	#1-55	Sealed Exp JT (4-inch) (SEJ-M)	.	1081	Linear Feet	\$ 325.00	\$ 351,325.00

Success: All values provided	#1-56	Pavement Markings and Signage	.	1	Lump Sum	\$ 72,000.00	\$ 72,000.00
Success: All values provided	#1-57	Trench Safety for Storm Drain Lines	.	4763	Linear Feet	\$ 1.00	\$ 4,763.00
Success: All values provided	#1-58	18-Inch Class III Reinforced Concrete Pipe	.	921	Linear Feet	\$ 237.00	\$ 218,277.00
Success: All values provided	#1-59	24-Inch Class III Reinforced Concrete Pipe	.	1766	Linear Feet	\$ 200.00	\$ 353,200.00
Success: All values provided	#1-60	30-Inch Class III Reinforced Concrete Pipe	.	1695	Linear Feet	\$ 254.44	\$ 431,275.80
Success: All values provided	#1-61	36-Inch Class III Reinforced Concrete Pipe	.	341	Linear Feet	\$ 348.00	\$ 118,668.00
Success: All values provided	#1-62	4 feet by 3 feet Class III Reinforced Concrete Box Extension	.	40	Linear Feet	\$ 902.00	\$ 36,080.00
Success: All values provided	#1-63	24-Inch CMP Temporary Culverts	.	160	Linear Feet	\$ 255.00	\$ 40,800.00
Success: All values provided	#1-64	Replace 10-foot Inlet Top	.	1	Each	\$ 6,780.00	\$ 6,780.00
Success: All values provided	#1-65	10-foot Recessed Curb Inlet	.	22	Each	\$ 15,000.00	\$ 330,000.00
Success: All values provided	#1-66	4-foot Square Storm Drain Manhole	.	8	Each	\$ 11,000.00	\$ 88,000.00
Success: All values provided	#1-67	Sloped Headwall for 30-Inch RCP	.	1	Each	\$ 4,675.00	\$ 4,675.00
Success: All values provided	#1-68	Sloped Headwall for 36-Inch RCP	.	2	Each	\$ 6,265.00	\$ 12,530.00
Success: All values provided	#1-69	Wingwall for 4 feet by 3 feet RCB (SET - Type 1)	.	2	Each	\$ 7,450.00	\$ 14,900.00
Success: All values provided	#1-70	18-Inch Grouted Rock Riprap	.	79	Cubic Yard	\$ 551.00	\$ 43,529.00
Success: All values provided	#1-71	2-Inch RM Conduit on Bridge	.	4461	Linear Feet	\$ 71.22	\$ 317,712.42
Success: All values provided	#1-72	2-Inch Conduit for Street Lights	.	6082	Linear Feet	\$ 30.52	\$ 185,622.64
Success: All values provided	#1-73	2-Inch Conduit for Irrigation	.	2184	Linear Feet	\$ 30.52	\$ 66,655.68
Success: All values provided	#1-74	Street Light Mount on Bridge	.	8	Each	\$ 585.04	\$ 4,680.32
Success: All values provided	#1-75	Street Light Foundation	.	12	Each	\$ 3,758.50	\$ 45,102.00
Success: All values provided	#1-76	Street Light Pole and Luminaire	.	12	Each	\$ 8,245.51	\$ 98,946.12
Success: All values provided	#1-77	Street Light Pole and Luminaire on Bridge	.	8	Each	\$ 6,194.31	\$ 49,554.48
Success: All values provided	#1-78	Street Light Ground Box	.	15	Each	\$ 1,257.58	\$ 18,863.70
Success: All values provided	#1-79	Irrigation Ground Box	.	4	Each	\$ 1,257.58	\$ 5,030.32
Success: All values provided	#1-80	No 8 AWG Insulated Stranded Copper Conductor	.	21086	Linear Feet	\$ 2.54	\$ 53,558.44
Success: All values provided	#1-81	No 8 AWG Bare Solid Copper Conductor	.	10543	Linear Feet	\$ 2.54	\$ 26,779.22
Success: All values provided	#1-82	No 12 AWG Insulated Stranded Copper Conductor	.	4368	Linear Feet	\$ 1.53	\$ 6,683.04
Success: All values provided	#1-83	No 12 AWG Bare Solid Copper Conductor	.	2184	Linear Feet	\$ 1.53	\$ 3,341.52
Success: All values provided	#1-84	Electrical Service for Lighting	.	1	Each	\$ 10,451.37	\$ 10,451.37
Success: All values provided	#1-85	Electrical Service for Irrigation	.	2	Each	\$ 9,085.94	\$ 18,171.88

Success: All values provided	#1-86	4-Inch SCH 40 PVC Irrigation Sleeve	.	87	Linear Feet	\$ 37.14	\$ 3,231.18
Success: All values provided	#1-87	6-Inch SCH 40 PVC Irrigation Sleeve	.	87	Linear Feet	\$ 51.89	\$ 4,514.43
Success: All values provided	#1-88	Ground Box TY D W/Apron	.	8	Each	\$ 2,637.26	\$ 21,098.08
Success: All values provided	#1-89	Conduit (PVC) (SCH 80) (4-Inch)	.	530	Linear Feet	\$ 44.77	\$ 23,728.10
Success: All values provided	#1-90	Conduit (PVC) (SCH 80) (4-Inch) (Bore)	.	315	Linear Feet	\$ 59.01	\$ 18,588.15
Success: All values provided	#1-91	Abandon Existing Water Line and Fill with Grout	.	434	Linear Feet	\$ 62.00	\$ 26,908.00
Success: All values provided	#1-92	Trench Safety for Water Lines and Sanitary Sewer Lines	.	11026	Linear Feet	\$ 1.00	\$ 11,026.00
Success: All values provided	#1-93	6-Inch PVC Water Line (Open Cut)	.	518	Linear Feet	\$ 130.00	\$ 67,340.00
Success: All values provided	#1-94	8-Inch PVC Water Line (Open Cut)	.	10	Linear Feet	\$ 587.00	\$ 5,870.00
Success: All values provided	#1-95	10-Inch PVC Water Line (Open Cut)	.	370	Linear Feet	\$ 176.00	\$ 65,120.00
Success: All values provided	#1-96	12-Inch PVC Water Line (Open Cut)	.	9415	Linear Feet	\$ 225.00	\$ 2,118,375.00
Success: All values provided	#1-97	12-Inch PVC Water Line (Open Cut) (Restrained Joints)	.	160	Linear Feet	\$ 231.00	\$ 36,960.00
Success: All values provided	#1-98	12-Inch PVC Water Line (In Casing)	.	221	Linear Feet	\$ 1,000.00	\$ 221,000.00
Success: All values provided	#1-99	20-Inch OD x 3/8-inch Thick Steel Casing (By Other Than Open Cut)	.	221	Linear Feet	\$ 290.00	\$ 64,090.00
Success: All values provided	#1-100	6-Inch Gate Valve	.	19	Each	\$ 5,215.00	\$ 99,085.00
Success: All values provided	#1-101	8-Inch Gate Valve	.	1	Each	\$ 5,775.00	\$ 5,775.00
Success: All values provided	#1-102	12-Inch Gate Valve	.	24	Each	\$ 8,900.00	\$ 213,600.00
Success: All values provided	#1-103	8-Inch Cut-In Tee and Gate Valve	.	1	Each	\$ 11,405.00	\$ 11,405.00
Success: All values provided	#1-104	Fire Hydrant Assembly	.	17	Each	\$ 8,205.00	\$ 139,485.00
Success: All values provided	#1-105	Connect to Existing Water Line	.	3	Each	\$ 7,025.00	\$ 21,075.00
Success: All values provided	#1-106	Blow-Off Valve	.	1	Each	\$ 13,005.00	\$ 13,005.00
Success: All values provided	#1-107	2-Inch Combination Air Valve Assembly	.	2	Each	\$ 15,000.00	\$ 30,000.00
Success: All values provided	#1-108	12-Inch by 12-Inch Tapping Sleeve and Gate Valve	.	1	Each	\$ 13,000.00	\$ 13,000.00
Success: All values provided	#1-109	12-Inch SCH 40 Welded Steel Water Pipe with Epoxy Lining/Coating	.	2073	Linear Feet	\$ 289.00	\$ 599,097.00
Success: All values provided	#1-110	Water Pipe Support and Hanger Single Roller	.	164	Each	\$ 810.00	\$ 132,840.00
Success: All values provided	#1-111	Water Pipe Support and Hanger Double Roller	.	41	Each	\$ 1,150.00	\$ 47,150.00
Success: All values provided	#1-112	Water Line Mechanical Expansion Joint for Straith Sections	.	3	Each	\$ 19,900.00	\$ 59,700.00
Success: All values provided	#1-113	Water Line Flexible Mechanical Joint for Curve Sections	.	3	Each	\$ 19,900.00	\$ 59,700.00

Success: All values provided	#1-114	Abandon Sanitary Sewer Line and Fill with Grout	.	569	Linear Feet	\$ 43.00	\$ 24,467.00
Success: All values provided	#1-115	Remove Existing Sanitary Sewer Manhole	.	1	Each	\$ 11,180.00	\$ 11,180.00
Success: All values provided	#1-116	24-Inch PS 115 PVC Sanitary Sewer	.	553	Linear Feet	\$ 332.00	\$ 183,596.00
Success: All values provided	#1-117	6-Foot Sanitary Sewer Manhole	.	3	Each	\$ 29,050.00	\$ 87,150.00
Success: All values provided	#1-118	6-Foot Sanitary Sewer Manhole Extra Depth	.	19	Vertical Feet	\$ 1,132.00	\$ 21,508.00
							\$ 40,989,997.00

**Alternate #1**

Success: All values provided	#2-1	ADDED COST FOR DELAYED START by CITY	.	3	Months	\$ 1.00	\$ 3.00
							\$40,990,000.00

**Responses**

**24-08-C Corporate Drive Segments 2 & 3**

Success: All data is valid!

Status	#	Item	Item Description	Quantity Required	Unit of Measure	Numeric Unit Price	Total Cost
--------	---	------	------------------	-------------------	-----------------	-----------------------	------------

**Corporate Drive Segments @ & 3**

Success: All values provided	#1-1	<b>Mobilization</b>	Start-up (Bonds, Insurance, Move-In, Sanitary Facilities) not to exceed 5%	1	Lump Sum	\$ 2,042,530.00	\$ 2,042,530.00
Success: All values provided	#1-2	<b>Temporary Traffic Control</b>	Includes Barriers, Barricades, and Detour Signs	1	Lump Sum	\$ 100,000.00	\$ 100,000.00
Success: All values provided	#1-3	<b>Prepare Right of Way</b>	.	84	Stations	\$ 25,000.00	\$ 2,100,000.00
Success: All values provided	#1-4	<b>Sawcut and Remove Concrete Flatwork</b>	Includes sawcutting, removal and disposal	4035	Square Feet	\$ 3.00	\$ 12,105.00
Success: All values provided	#1-5	<b>Sawcut and Remove Curb and Gutter</b>	Includes sawcutting, removal and disposal	529	Linear Feet	\$ 7.50	\$ 3,967.50
Success: All values provided	#1-6	<b>Remove Gravel Pavement</b>	.	3494	Square Feet	\$ 1.15	\$ 4,018.10
Success: All values provided	#1-7	<b>Remove Fences (All Types)</b>	.	325	Linear Feet	\$ 5.30	\$ 1,722.50
Success: All values provided	#1-8	<b>Remove Headwall (for 4 feet by 3 feet Box Culvert)</b>	.	2	Each	\$ 2,245.00	\$ 4,490.00
Success: All values provided	#1-9	<b>Unclassified Excavation</b>	.	22973	Cubic Yard	\$ 12.18	\$ 279,811.14
Success: All values provided	#1-10	<b>Borrow from Mitigation Site</b>	.	68556	Cubic Yard	\$ 15.00	\$ 1,028,340.00
Success: All values provided	#1-11	<b>Install Barbed Wire Fence</b>	.	59	Linear Feet	\$ 20.00	\$ 1,180.00
Success: All values provided	#1-12	<b>Clearing and Grubbing</b>	.	30	Acres	\$ 3,500.00	\$ 105,000.00
Success: All values provided	#1-13	<b>Heavy Tree Clearing and Grubbing</b>	.	11.51	Acres	\$ 9,072.00	\$ 104,418.72
Success: All values provided	#1-14	<b>Erosion Control (SWPPP) and Removal</b>	.	1	Lump Sum	\$ 350,000.00	\$ 350,000.00
Success: All values provided	#1-15	<b>4-Inch Topsoil and Hydromulch Seeding</b>	.	54300	Square Yard	\$ 2.18	\$ 118,374.00
Success: All values provided	#1-16	<b>4-Inch Topsoil and Seeding with Soil Retention Blanket</b>	.	59613	Square Yard	\$ 3.20	\$ 190,761.60
Success: All values provided	#1-17	<b>Concrete Riprap Type RR8</b>	.	1581	Square Yard	\$ 135.00	\$ 213,435.00
Success: All values provided	#1-18	<b>6-Inch Lime Stabilized Subgrade</b>	.	38284	Square Yard	\$ 8.90	\$ 340,727.60
Success: All values provided	#1-19	<b>Furnish Lime (36 pounds per square yard)</b>	.	688	Ton	\$ 301.00	\$ 207,088.00
Success: All values provided	#1-20	<b>8-Inch Flex Base</b>	.	3807	Square Yard	\$ 24.00	\$ 91,368.00
Success: All values provided	#1-21	<b>10-Inch Reinforced Concrete Pavement Including 6-Inch Curb</b>	.	35753	Square Yard	\$ 115.00	\$ 4,111,595.00
Success: All values provided	#1-22	<b>Concrete Header</b>	.	100	Linear Feet	\$ 82.00	\$ 8,200.00
Success: All values provided	#1-23	<b>Anchorage Joint</b>	.	968	Linear Feet	\$ 18.00	\$ 17,424.00
Success: All values provided	#1-24	<b>Sawtooth Curb and Gutter</b>	.	90	Linear Feet	\$ 80.00	\$ 7,200.00
Success: All values provided	#1-25	<b>Concrete Curb and Gutter</b>	.	133	Linear Feet	\$ 64.00	\$ 8,512.00
Success: All values provided	#1-26	<b>5-Inch Thick Concrete Sidewalk</b>	.	102255	Square Feet	\$ 10.00	\$ 1,022,550.00
Success: All values provided	#1-27	<b>Stamped Concrete Median Pavement</b>	.	1519	Square Feet	\$ 26.00	\$ 39,494.00
Success: All values provided	#1-28	<b>6-Inch Gravel Driveway</b>	.	307	Square Yard	\$ 32.00	\$ 9,824.00

Success: All values provided	#1-29	Barrier-Free Sidewalk Ramp -12-Foot Modified Type 3 (5-Inch Thick Concrete ADA Ramp)	.	3	Each	\$ 6,500.00	\$ 19,500.00
Success: All values provided	#1-30	Barrier-Free Sidewalk Ramp -5-Foot Modified Type 7 (5-Inch Thick Concrete ADA Ramp)	.	1	Each	\$ 4,700.00	\$ 4,700.00
Success: All values provided	#1-31	Barrier-Free Sidewalk Ramp -6-Foot Modified Type 7 (5-Inch Thick Concrete ADA Ramp)	.	2	Each	\$ 4,900.00	\$ 9,800.00
Success: All values provided	#1-32	Barrier-Free Sidewalk Ramp -12-Foot Modified Type 7 (5-Inch Thick Concrete ADA Ramp)	.	1	Each	\$ 7,800.00	\$ 7,800.00
Success: All values provided	#1-33	Barrier-Free Sidewalk Ramp -12-Foot Modified Type 21 (5-Inch Thick Concrete ADA Ramp)	.	1	Each	\$ 6,500.00	\$ 6,500.00
Success: All values provided	#1-34	Barrier-Free Sidewalk Ramp -5-Foot Modified Type 21 (5-Inch Thick Concrete ADA Ramp)	.	1	Each	\$ 5,100.00	\$ 5,100.00
Success: All values provided	#1-35	MSE Retaining Wall	.	4004	Square Feet	\$ 140.00	\$ 560,560.00
Success: All values provided	#1-36	Anti-Graffiti Xcoating (Permanent-TY III)	.	4004	Square Feet	\$ 1.20	\$ 4,804.80
Success: All values provided	#1-37	Type C402 (HPC) Traffic Rail	.	628	Linear Feet	\$ 300.00	\$ 188,400.00
Success: All values provided	#1-38	Traffic Rail Transition	.	160	Linear Feet	\$ 223.00	\$ 35,680.00
Success: All values provided	#1-39	Pedestrian Rail with Coping	.	20	Linear Feet	\$ 799.00	\$ 15,980.00
Success: All values provided	#1-40	Pedestrian Rail with Coping Transition	.	10	Linear Feet	\$ 850.00	\$ 8,500.00
Success: All values provided	#1-41	Drill Shaft (24-Inch)	.	657	Linear Feet	\$ 128.00	\$ 84,096.00
Success: All values provided	#1-42	Drill Shaft (36-Inch)	.	13526	Linear Feet	\$ 261.50	\$ 3,537,049.00
Success: All values provided	#1-43	Drill Shaft (42-Inch)	.	1088	Linear Feet	\$ 374.00	\$ 406,912.00
Success: All values provided	#1-44	Class C Concrete (ABUT) (HPC)	.	211	Cubic Yard	\$ 1,500.00	\$ 316,500.00
Success: All values provided	#1-45	Class C Concrete (CAP) (HPC)	.	654.4	Cubic Yard	\$ 1,815.00	\$ 1,187,736.00
Success: All values provided	#1-46	Class C Concrete (COL) (HPC)	.	381.5	Cubic Yard	\$ 2,161.00	\$ 824,421.50
Success: All values provided	#1-47	Class S Concrete (BAS) (HPC)	.	354.2	Cubic Yard	\$ 810.00	\$ 286,902.00
Success: All values provided	#1-48	Class S Concrete (Median) (HPC)	.	324.3	Cubic Yard	\$ 707.00	\$ 229,280.10
Success: All values provided	#1-49	Reinforced Concrete Slab Class S (HPC)	.	174153	Square Feet	\$ 28.00	\$ 4,876,284.00
Success: All values provided	#1-50	Prestress Concrete Girder (Tx46)	.	19620.28	Linear Feet	\$ 270.00	\$ 5,297,475.60
Success: All values provided	#1-51	Prestress Concrete Girder (Tx54)	.	1894.34	Linear Feet	\$ 312.00	\$ 591,034.08
Success: All values provided	#1-52	Structural Steel (Misc. Non-Bridge) (BS-EJCP) (TXDOT Item 442)	.	3040	Pounds	\$ 26.00	\$ 79,040.00
Success: All values provided	#1-53	C402 Rail (HPC) (Bridge)	.	4378	Linear Feet	\$ 330.00	\$ 1,444,740.00
Success: All values provided	#1-54	Rail (Custom Pedestrian Rail)	.	4380	Linear Feet	\$ 465.00	\$ 2,036,700.00
Success: All values provided	#1-55	Sealed Exp JT (4-inch) (SEJ-M)	.	1081	Linear Feet	\$ 336.00	\$ 363,216.00
Success: All values provided	#1-56	Pavement Markings and Signage	.	1	Lump Sum	\$ 72,000.00	\$ 72,000.00
Success: All values provided	#1-57	Trench Safety for Storm Drain Lines	.	4763	Linear Feet	\$ 1.00	\$ 4,763.00
Success: All values provided	#1-58	18-Inch Class III Reinforced Concrete Pipe	.	921	Linear Feet	\$ 245.00	\$ 225,645.00
Success: All values provided	#1-59	24-Inch Class III Reinforced Concrete Pipe	.	1766	Linear Feet	\$ 210.00	\$ 370,860.00

Success: All values provided	#1-60	30-Inch Class III Reinforced Concrete Pipe	.	1695	Linear Feet	\$ 265.00	\$ 449,175.00
Success: All values provided	#1-61	36-Inch Class III Reinforced Concrete Pipe	.	341	Linear Feet	\$ 362.00	\$ 123,442.00
Success: All values provided	#1-62	4 feet by 3 feet Class III Reinforced Concrete Box Extension	.	40	Linear Feet	\$ 935.00	\$ 37,400.00
Success: All values provided	#1-63	24-Inch CMP Temporary Culverts	.	160	Linear Feet	\$ 265.00	\$ 42,400.00
Success: All values provided	#1-64	Replace 10-foot Inlet Top	.	1	Each	\$ 7,000.00	\$ 7,000.00
Success: All values provided	#1-65	10-foot Recessed Curb Inlet	.	22	Each	\$ 15,000.00	\$ 330,000.00
Success: All values provided	#1-66	4-foot Square Storm Drain Manhole	.	8	Each	\$ 11,000.00	\$ 88,000.00
Success: All values provided	#1-67	Sloped Headwall for 30-Inch RCP	.	1	Each	\$ 4,825.00	\$ 4,825.00
Success: All values provided	#1-68	Sloped Headwall for 36-Inch RCP	.	2	Each	\$ 6,480.00	\$ 12,960.00
Success: All values provided	#1-69	Wingwall for 4 feet by 3 feet RCB (SET - Type 1)	.	2	Each	\$ 7,720.00	\$ 15,440.00
Success: All values provided	#1-70	18-Inch Grouted Rock Riprap	.	79	Cubic Yard	\$ 570.00	\$ 45,030.00
Success: All values provided	#1-71	2-Inch RM Conduit on Bridge	.	4461	Linear Feet	\$ 71.22	\$ 317,712.42
Success: All values provided	#1-72	2-Inch Conduit for Street Lights	.	6082	Linear Feet	\$ 30.52	\$ 185,622.64
Success: All values provided	#1-73	2-Inch Conduit for Irrigation	.	2184	Linear Feet	\$ 30.52	\$ 66,655.68
Success: All values provided	#1-74	Street Light Mount on Bridge	.	8	Each	\$ 585.04	\$ 4,680.32
Success: All values provided	#1-75	Street Light Foundation	.	12	Each	\$ 3,758.50	\$ 45,102.00
Success: All values provided	#1-76	Street Light Pole and Luminaire	.	12	Each	\$ 8,245.51	\$ 98,946.12
Success: All values provided	#1-77	Street Light Pole and Luminaire on Bridge	.	8	Each	\$ 6,194.31	\$ 49,554.48
Success: All values provided	#1-78	Street Light Ground Box	.	15	Each	\$ 1,257.58	\$ 18,863.70
Success: All values provided	#1-79	Irrigation Ground Box	.	4	Each	\$ 1,257.58	\$ 5,030.32
Success: All values provided	#1-80	No 8 AWG Insulated Stranded Copper Conductor	.	21086	Linear Feet	\$ 2.54	\$ 53,558.44
Success: All values provided	#1-81	No 8 AWG Bare Solid Copper Conductor	.	10543	Linear Feet	\$ 2.54	\$ 26,779.22
Success: All values provided	#1-82	No 12 AWG Insulated Stranded Copper Conductor	.	4368	Linear Feet	\$ 1.53	\$ 6,683.04
Success: All values provided	#1-83	No 12 AWG Bare Solid Copper Conductor	.	2184	Linear Feet	\$ 1.53	\$ 3,341.52
Success: All values provided	#1-84	Electrical Service for Lighting	.	1	Each	\$ 10,451.37	\$ 10,451.37
Success: All values provided	#1-85	Electrical Service for Irrigation	.	2	Each	\$ 9,085.94	\$ 18,171.88
Success: All values provided	#1-86	4-Inch SCH 40 PVC Irrigation Sleeve	.	87	Linear Feet	\$ 37.14	\$ 3,231.18
Success: All values provided	#1-87	6-Inch SCH 40 PVC Irrigation Sleeve	.	87	Linear Feet	\$ 51.89	\$ 4,514.43
Success: All values provided	#1-88	Ground Box TY D W/Apron	.	8	Each	\$ 2,637.26	\$ 21,098.08
Success: All values provided	#1-89	Conduit (PVC) (SCH 80) (4-Inch)	.	530	Linear Feet	\$ 44.77	\$ 23,728.10
Success: All values provided	#1-90	Conduit (PVC) (SCH 80) (4-Inch) (Bore)	.	315	Linear Feet	\$ 59.01	\$ 18,588.15
Success: All values provided	#1-91	Abandon Existing Water Line and Fill with Grout	.	434	Linear Feet	\$ 64.00	\$ 27,776.00

Success: All values provided	#1-92	Trench Safety for Water Lines and Sanitary Sewer Lines	.	11026	Linear Feet	\$ 1.00	\$ 11,026.00
Success: All values provided	#1-93	6-Inch PVC Water Line (Open Cut)	.	518	Linear Feet	\$ 134.00	\$ 69,412.00
Success: All values provided	#1-94	8-Inch PVC Water Line (Open Cut)	.	10	Linear Feet	\$ 601.00	\$ 6,010.00
Success: All values provided	#1-95	10-Inch PVC Water Line (Open Cut)	.	370	Linear Feet	\$ 180.00	\$ 66,600.00
Success: All values provided	#1-96	12-Inch PVC Water Line (Open Cut)	.	9415	Linear Feet	\$ 235.00	\$ 2,212,525.00
Success: All values provided	#1-97	12-Inch PVC Water Line (Open Cut) (Restrained Joints)	.	160	Linear Feet	\$ 240.00	\$ 38,400.00
Success: All values provided	#1-98	12-Inch PVC Water Line (In Casing)	.	221	Linear Feet	\$ 1,021.00	\$ 225,641.00
Success: All values provided	#1-99	20-Inch OD x 3/8-inch Thick Steel Casing (By Other Than Open Cut)	.	221	Linear Feet	\$ 290.00	\$ 64,090.00
Success: All values provided	#1-100	6-Inch Gate Valve	.	19	Each	\$ 5,400.00	\$ 102,600.00
Success: All values provided	#1-101	8-Inch Gate Valve	.	1	Each	\$ 6,000.00	\$ 6,000.00
Success: All values provided	#1-102	12-Inch Gate Valve	.	24	Each	\$ 9,200.00	\$ 220,800.00
Success: All values provided	#1-103	8-Inch Cut-In Tee and Gate Valve	.	1	Each	\$ 11,800.00	\$ 11,800.00
Success: All values provided	#1-104	Fire Hydrant Assembly	.	17	Each	\$ 8,490.00	\$ 144,330.00
Success: All values provided	#1-105	Connect to Existing Water Line	.	3	Each	\$ 7,271.00	\$ 21,813.00
Success: All values provided	#1-106	Blow-Off Valve	.	1	Each	\$ 13,005.00	\$ 13,005.00
Success: All values provided	#1-107	2-Inch Combination Air Valve Assembly	.	2	Each	\$ 15,000.00	\$ 30,000.00
Success: All values provided	#1-108	12-Inch by 12-Inch Tapping Sleeve and Gate Valve	.	1	Each	\$ 13,268.00	\$ 13,268.00
Success: All values provided	#1-109	12-Inch SCH 40 Welded Steel Water Pipe with Epoxy Lining/Coating	.	2073	Linear Feet	\$ 300.00	\$ 621,900.00
Success: All values provided	#1-110	Water Pipe Support and Hanger Single Roller	.	164	Each	\$ 830.00	\$ 136,120.00
Success: All values provided	#1-111	Water Pipe Support and Hanger Double Roller	.	41	Each	\$ 1,190.00	\$ 48,790.00
Success: All values provided	#1-112	Water Line Mechanical Expansion Joint for Straith Sections	.	3	Each	\$ 21,000.00	\$ 63,000.00
Success: All values provided	#1-113	Water Line Flexible Mechanical Joint for Curve Sections	.	3	Each	\$ 21,000.00	\$ 63,000.00
Success: All values provided	#1-114	Abandon Sanitary Sewer Line and Fill with Grout	.	569	Linear Feet	\$ 45.00	\$ 25,605.00
Success: All values provided	#1-115	Remove Existing Sanitary Sewer Manhole	.	1	Each	\$ 11,575.00	\$ 11,575.00
Success: All values provided	#1-116	24-Inch PS 115 PVC Sanitary Sewer	.	553	Linear Feet	\$ 344.00	\$ 190,232.00
Success: All values provided	#1-117	6-Foot Sanitary Sewer Manhole	.	3	Each	\$ 30,000.00	\$ 90,000.00
Success: All values provided	#1-118	6-Foot Sanitary Sewer Manhole Extra Depth	.	19	Vertical Feet	\$ 1,150.00	\$ 21,850.00

\$ 42,321,268.33

**Alternate #1**

Success: All values provided	#2-1	ADDED COST FOR DELAYED START by CITY	.	3	Months	\$ 1.00	\$ 3.00
------------------------------	------	--------------------------------------	---	---	--------	---------	---------

\$42,321,271.33

**BID BOND**

**STATE OF TEXAS  
COUNTY OF DENTON**

**KNOW ALL MEN BY THESE PRESENTS**, that we, the undersigned, Sundt Construction, Inc., whose address is 714 South Santa Rosa, San Antonio, TX 78204, hereinafter called Principal, and Zurich American Insurance Company, a corporation organized and existing under the laws of the State of New York, and fully licensed to transact business in the State of Texas, as Surety, are held and firmly bound unto the City of Lewisville, a home-rule municipal corporation organized and existing under the laws of the State of Texas, hereinafter referred to as "Owner," in the penal sum of \$Five Percent (5%)\* as the proper measure of liquidated damages arising out of or connected with the submission of a Bid Proposal for the construction of a public work project, in lawful money of the United States, to be paid in Denton County, Texas, for the payment of which sum well and truly to be made, we bind ourselves, our heirs, executors, administrators and successors jointly and severally, firmly by these presents. The condition of the above obligation is such that whereas the Principal has submitted to Owner a certain Bid Proposal, attached hereto and hereby made a part hereof, to enter into a contract in writing, for the **Corporate Drive Segments 2 & 3** the "Project").  
\*of Principal's Bid Amount

**NOW, THEREFORE**, if the Principal's Proposal shall be rejected or, in the alternative, if the Principal's Proposal shall be accepted and the Principal shall execute and deliver a contract in the form of the Contract attached hereto (properly completed in accordance with said Proposal) and shall furnish performance, payment and maintenance bonds required by the Contract Documents for the Project and provide proof of all required insurance coverages for the Project and shall in all other respects perform the agreement created by the acceptance of said Proposal, then this obligation shall be void, otherwise the same shall remain in force and affect; it being expressly understood and agreed that the liability of the Surety for any breach of condition hereunder shall be in the face amount of this bond and forfeited as a proper measure of liquidated damages.

**PROVIDED FURTHER**, that if any legal action were filed on this Bond, exclusive Venue shall lie in Denton County, Texas.

**AND PROVIDED FURTHER**, the Surety, for value received, hereby stipulates and agrees that the obligations of said Surety and its bond shall be in no way impaired or affected by an extension of the time within which the Owner may accept such Proposal; and said Surety does hereby waive notice of any such extension.

The undersigned and designated agent is hereby designated by the Surety herein as the Resident Agent in Denton County or Dallas County to whom any requisite notices may be delivered and on whom service of process may be had in matters arising out of such suretyship, as provided by Article 7.19-1 of the Insurance Code, Vernon's Annotated Civil Statutes of the State of Texas.

**IN WITNESS WHEREOF**, this instrument is executed in One (1) copies, each one of which shall be deemed an original, this, the 6th day of November, 2023.

ATTEST:

By: Michelle R. Arnesen  
 Signature  
Michelle R. Arnesen  
 Typed/Printed Name  
Assistant Corporate Secretary  
 Title  
2620 S. 55th Street  
 Address  
Tempe AZ 85282  
 City State Zip  
480-293-3000  
 Phone Fax

PRINCIPAL:

Sundt Construction, Inc.  
 Company Name  
 By: Jeff Williamson  
 Signature  
JEFF WILLIAMSON  
 Typed/Printed Name  
PRESIDENT - TRANSPORTATION  
 Title  
2620 S. 55th St.  
 Address  
TEMPE AZ 85282  
 City State Zip  
480 293-3000  
 Phone Fax

ATTEST:

By: Debbie Clayton  
 Signature  
Debbie Clayton  
 Printed Name  
 Witness  
 Title  
8605 E Raintree Drive, Suite 200  
 Address  
Scottsdale AZ 85260  
 City State Zip  
(602) 956-2250 (602) 956-2258  
 Phone Fax

SURETY: Zurich American Insurance Company

By: Alexandra Haas  
 Signature  
Alexandra Haas  
 Printed Name  
 Attorney-In-Fact  
 Title  
1299 Zurich Way, 5th Floor  
 Address  
Schaumburg IL 60196  
 City State Zip  
(847) 605-6000 (877) 962-2567  
 Phone Fax

The Resident Agent of the Surety in Denton County or Dallas County, Texas, for delivery of notice and service of the process is:

NAME: Jennifer Winters, Arthur J. Gallagher Risk Management Services, LLC  
 STREET ADDRESS: 12750 Merit Drive, Suite 1000  
 CITY, STATE, ZIP: Dallas, TX 75251

**NOTE:** If Resident Agent is not a corporation, give a person's name.

**ZURICH AMERICAN INSURANCE COMPANY  
COLONIAL AMERICAN CASUALTY AND SURETY COMPANY  
FIDELITY AND DEPOSIT COMPANY OF MARYLAND  
POWER OF ATTORNEY**

KNOW ALL MEN BY THESE PRESENTS: That the ZURICH AMERICAN INSURANCE COMPANY, a corporation of the State of New York, the COLONIAL AMERICAN CASUALTY AND SURETY COMPANY, a corporation of the State of Illinois, and the FIDELITY AND DEPOSIT COMPANY OF MARYLAND a corporation of the State of Illinois (herein collectively called the "Companies"), by Robert D. Murray, Vice President, in pursuance of authority granted by Article V, Section 8, of the By-Laws of said Companies, which are set forth on the reverse side hereof and are hereby certified to be in full force and effect on the date hereof, do hereby nominate, constitute, and appoint **Debbie CLAYTON, Brian D. WILDER, Holly BYRD, Alexandra HAAS, Randi L MORGAN and Tina Marie PERKINS of Tempe, Arizona**, its true and lawful agent and Attorney-in-Fact, to make, execute, seal and deliver, for, and on its behalf as surety, and as its act and deed: **any and all bonds and undertakings**, and the execution of such bonds or undertakings in pursuance of these presents, shall be as binding upon said Companies, as fully and amply, to all intents and purposes, as if they had been duly executed and acknowledged by the regularly elected officers of the ZURICH AMERICAN INSURANCE COMPANY at its office in New York, New York., the regularly elected officers of the COLONIAL AMERICAN CASUALTY AND SURETY COMPANY at its office in Owings Mills, Maryland., and the regularly elected officers of the FIDELITY AND DEPOSIT COMPANY OF MARYLAND at its office in Owings Mills, Maryland., in their own proper persons.

The said Vice President does hereby certify that the extract set forth on the reverse side hereof is a true copy of Article V, Section 8, of the By-Laws of said Companies, and is now in force.

IN WITNESS WHEREOF, the said Vice-President has hereunto subscribed his/her names and affixed the Corporate Seals of the said **ZURICH AMERICAN INSURANCE COMPANY, COLONIAL AMERICAN CASUALTY AND SURETY COMPANY, and FIDELITY AND DEPOSIT COMPANY OF MARYLAND**, this 13th day of April, A.D. 2023.



**ATTEST:**  
**ZURICH AMERICAN INSURANCE COMPANY  
COLONIAL AMERICAN CASUALTY AND SURETY COMPANY  
FIDELITY AND DEPOSIT COMPANY OF MARYLAND**

By: *Robert D. Murray*  
Vice President

By: *Dawn E. Brown*  
Secretary

**State of Maryland  
County of Baltimore**

On this 13th day of April A.D. 2023, before the subscriber, a Notary Public of the State of Maryland, duly commissioned and qualified, **Robert D. Murray, Vice President and Dawn E. Brown, Secretary** of the Companies, to me personally known to be the individuals and officers described in and who executed the preceding instrument, and acknowledged the execution of same, and being by me duly sworn, depose and saith, that he/she is the said officer of the Company aforesaid, and that the seals affixed to the preceding instrument are the Corporate Seals of said Companies, and that the said Corporate Seals and the signature as such officer were duly affixed and subscribed to the said instrument by the authority and direction of the said Corporations.

IN TESTIMONY WHEREOF, I have hereunto set my hand and affixed my Official Seal the day and year first above written.

Iva Betha  
Notary Public  
My Commission Expires September 30, 2023



**EXTRACT FROM BY-LAWS OF THE COMPANIES**

"Article V, Section 8, Attorneys-in-Fact. The Chief Executive Officer, the President, or any Executive Vice President or Vice President may, by written instrument under the attested corporate seal, appoint attorneys-in-fact with authority to execute bonds, policies, recognizances, stipulations, undertakings, or other like instruments on behalf of the Company, and may authorize any officer or any such attorney-in-fact to affix the corporate seal thereto; and may with or without cause modify or revoke any such appointment or authority at any time."

**CERTIFICATE**

I, the undersigned, Vice President of the ZURICH AMERICAN INSURANCE COMPANY, the COLONIAL AMERICAN CASUALTY AND SURETY COMPANY, and the FIDELITY AND DEPOSIT COMPANY OF MARYLAND, do hereby certify that the foregoing Power of Attorney is still in full force and effect on the date of this certificate; and I do further certify that Article V, Section 8, of the By-Laws of the Companies is still in force.

This Power of Attorney and Certificate may be signed by facsimile under and by authority of the following resolution of the Board of Directors of the ZURICH AMERICAN INSURANCE COMPANY at a meeting duly called and held on the 15th day of December 1998.

RESOLVED: "That the signature of the President or a Vice President and the attesting signature of a Secretary or an Assistant Secretary and the Seal of the Company may be affixed by facsimile on any Power of Attorney...Any such Power or any certificate thereof bearing such facsimile signature and seal shall be valid and binding on the Company."

This Power of Attorney and Certificate may be signed by facsimile under and by authority of the following resolution of the Board of Directors of the COLONIAL AMERICAN CASUALTY AND SURETY COMPANY at a meeting duly called and held on the 5th day of May, 1994, and the following resolution of the Board of Directors of the FIDELITY AND DEPOSIT COMPANY OF MARYLAND at a meeting duly called and held on the 10th day of May, 1990.

RESOLVED: "That the facsimile or mechanically reproduced seal of the company and facsimile or mechanically reproduced signature of any Vice-President, Secretary, or Assistant Secretary of the Company, whether made heretofore or hereafter, wherever appearing upon a certified copy of any power of attorney issued by the Company, shall be valid and binding upon the Company with the same force and effect as though manually affixed.

IN TESTIMONY WHEREOF, I have hereunto subscribed my name and affixed the corporate seals of the said Companies, this 6th day of November, 2023.



*MJ Pethick*  
By: Mary Jean Pethick  
Vice President

**TO REPORT A CLAIM WITH REGARD TO A SURETY BOND, PLEASE SUBMIT A COMPLETE DESCRIPTION OF THE CLAIM INCLUDING THE PRINCIPAL ON THE BOND, THE BOND NUMBER, AND YOUR CONTACT INFORMATION TO:**

Zurich Surety Claims  
1299 Zurich Way  
Schaumburg, IL 60196-1056  
[reportsfclaims@zurichna.com](mailto:reportsfclaims@zurichna.com)  
800-626-4577

Authenticity of this bond can be confirmed at [bondvalidator.zurichna.com](http://bondvalidator.zurichna.com) or 410-559-8790

## Texas Important Notice

### IMPORTANT NOTICE

To obtain information or make a complaint:

You may call Zurich North America's toll-free telephone number for information or to make a complaint at:

**1-800-382-2150**

You may contact the Texas Department of Insurance to obtain information on companies, coverages, rights, or complaints at:

**1-800-252-3439**

You may write the Texas Department of Insurance:

P.O. Box 149104

Austin, TX 78714-9104

Fax: (512) 490-1007

Web: [www.tdi.texas.gov](http://www.tdi.texas.gov)

E-mail: [ConsumerProtection@tdi.texas.gov](mailto:ConsumerProtection@tdi.texas.gov)

### PREMIUM OR CLAIM DISPUTES:

Should you have a dispute concerning your premium or about a claim, you should contact the company first. If the dispute is not resolved, you may contact the Texas Department of Insurance.

### ATTACH THIS NOTICE TO YOUR POLICY:

This notice is for information only and does not become a part or condition of the attached document.

### AVISO IMPORTANTE

Para obtener información o para presentar una queja:

Usted puede llamar al número de teléfono gratuito de Zurich North America's para obtener información o para presentar una queja al:

**1-800-382-2150**

Usted puede comunicarse con el Departamento de Seguros de Texas para obtener información sobre compañías, coberturas, derechos, o quejas al:

**1-800-252-3439**

Usted puede escribir al Departamento de Seguros de Texas a:

P.O. Box 149104

Austin, TX 78714-9104

Fax: (512) 490-1007

Sitio web: [www.tdi.texas.gov](http://www.tdi.texas.gov)

E-mail: [ConsumerProtection@tdi.texas.gov](mailto:ConsumerProtection@tdi.texas.gov)

### DISPUTAS POR PRIMAS DE SEGUROS O RECLAMACIONES:

Si tiene una disputa relacionada con su prima de seguro o con una reclamación, usted debe comunicarse con la compañía primero. Si la disputa no es resuelta, usted puede comunicarse con el Departamento de Seguros de Texas.

**ADJUNTE ESTE AVISO A SU PÓLIZA:** Este aviso es solamente para propósitos informativos y no se convierte en parte o en condición del documento adjunto.

**PROPOSAL**

City of Lewisville  
Purchasing Office  
151 West Church Street  
P.O. Box 299002  
Lewisville, Texas 75029-9002

**CORPORATE DRIVE SEGMENTS 2 & 3  
PROJECT NO. G1122 & G1123**

Proposal of Sundt Construction, Inc.  
(hereinafter called Bidder), a corporation organized and existing under the laws of the State of  
Arizona. \_\_\_\_\_, ~~a partnership, or an individual doing business as~~

\_\_\_\_\_  
(Strike out inapplicable terms).

To the City of Lewisville, Texas (Owner)

The undersigned Bidder, in response to the Notice to Bidders for the construction of the above project and in conformity with the bidding documents; having examined the plans, specifications, related documents and the site of the proposed work; being familiar with all of the conditions relating to the construction of the proposed project, including the availability of materials and labor, hereby proposes to furnish all labor, materials, supplies, equipment, staking, testing, traffic control, superintendence, etc., for the construction of the project in accordance with the plans, specifications, and contract documents at the unit prices proposed herein.

The undersigned Bidder proposes, acknowledges and agrees to construct the entire project as shown on the plans, fully in accordance with the requirements of the plans, specifications, and the contract documents for the prices included in this Proposal and fully understands and agrees that the various items of material, labor and construction not specifically enumerated and provided for herein are considered subsidiary to the several items for which direct payment is specifically provided. Further, the undersigned agrees that one such subsidiary item is the protection, adjustment, maintenance, repair or replacement of all underground lines and services, whether shown on the plans or not, all to the full satisfaction of the City Engineer in a timely manner.

The undersigned Bidder agrees to begin work under the contract on or before the date specified in the written Notice to Proceed, and to fully complete the project within **915 calendar days**. These calendar days are assuming 6 days per week. It is specifically stated and understood that the entire construction including clean up shall be completed within the above stated time.

The undersigned Bidder has contacted, within 168 hours (1 week) prior to the bid opening, the Office of the City Engineer (972) 219-3490, and has determined that all Addenda are as follows:

Addendum No. 1 dated	<u>11.3.2023</u>	 _____ (Signature)
Addendum No. 2 dated	_____	_____ (Signature)
Addendum No. 3 dated	_____	_____ (Signature)

The undersigned Bidder acknowledges that the Owner reserves the right to waive any informality and to reject any or all proposals.

The undersigned Bidder acknowledges and agrees that this Proposal shall be good and may not be withdrawn for 120 days from the date of bid opening.

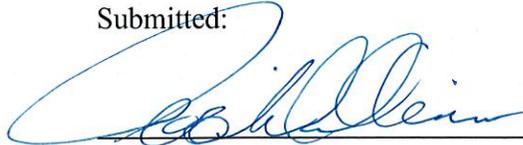
The undersigned Bidder has shown unit prices and amounts and agrees that in the case of discrepancy, the unit prices shown in figures shall stand and that the amounts and total will be adjusted to correspond to the unit prices shown.

The undersigned Bidder agrees to execute the Agreement and furnish the required Performance Bond and Payment Bond within fifteen calendar days from the date of award of a contract by the City; and agrees that any delay in furnishing the signed Agreement and Bonds will result in liquidated damages being applied in accordance with Item 108.8.1. of the Standard Specifications.

The undersigned Bidder has attached and made a part of this Proposal a bid security in conformance with Item 102.5. of the Standard Specifications and in accordance with the Notice to Bidders.

A 5% contingency line item will be included with the resulting contract and purchase order for this project. The contingency shall be used at the City's discretion and only upon written approval from the City. The amount listed as a contingency is not an obligation for payment from the City. Any unused contingency is retained by the City and is not payable to the Contractor.

Submitted:

  
\_\_\_\_\_  
(Signature)

Jeff J. Williamson  
\_\_\_\_\_  
(Name - Typed or Printed)

Group President  
\_\_\_\_\_  
(Title)

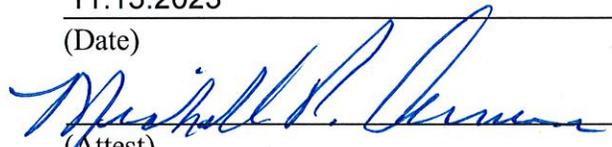
Sundt Construction, Inc.  
\_\_\_\_\_  
(Firm Name)

2620 S. 55th Street  
\_\_\_\_\_  
(Address)

Tempe, Maricopa, AZ 85282  
\_\_\_\_\_  
(City/County/State/Zip Code)

480.293.3237  
\_\_\_\_\_  
(Telephone Number/Include Area Code)

11.15.2023  
\_\_\_\_\_  
(Date)

  
\_\_\_\_\_  
(Attest)

(Seal, if corporation)

## **CONTRACTOR'S QUALIFICATIONS**

**ALL BIDDERS ARE NOTIFIED THAT THE FOLLOWING QUALIFICATION STATEMENT MUST BE COMPLETED AND SUBMITTED WITH THE BID OR PROPOSAL.**

The contractor shall show they have experience with similar projects that require working in confined areas in close proximity to many physical features (fences, utility poles, guy lines, gas lines and meters, sewer manholes, cleanouts, etc.) which requires the contractor to plan work efforts and equipment needs with these limitations in mind. The contractor shall submit a list of Public Works Projects successfully completed within the last five (5) years, including the names of supervisors and type of equipment used to perform this work.

**BIDDER'S QUALIFICATION STATEMENT**

Project: City of Lewisville, Corporate Drive Segment 2 & 3

Contractor: Sundt Construction, Inc.

Name: Jeff J. Williamson Title: Group President

Address: 8445 Freeport Parkway, Suite 240, Irving, TX 75063

Phone: 480.886.6600 Email: jjwilliams@sundt.com

**Type of Business:**

Sole Proprietor  Partnership  Corporation  Joint Venture Other \_\_\_\_\_

State and Date of Incorporation, Partnership, Ownership, Etc. Arizona, 1998

Principal Office Address: 2620 S. 55th Street, Tempe, AZ 85282

Principal Office Contact & Phone: Jeff J. Williamson, 480.293.3237

**Insurance:**

Insurance Agency Name: Lovitt & Touche (Marsh & McLennan Insurance Agency)

Insurance Agency Address: 1050 W Washington St, Ste. 233, Tempe, AZ 85281

Contact Name: Paul J. Lee, CRIS & Principal Phone: 602.778.7029

Liability Insurance Provided and Limits of Coverage: \$25,000,000

Workers Compensation Insurance Provider: Zurich American Insurance Company

Surety Bonding Company (Performance & Payment): Zurich American Insurance Company and Federal Insurance Company

**Total Number of Employees to be Associated with this Job:**

8 Managerial 1 Administrative \_\_\_\_\_ Professional

35 Skilled 13 Semi-Skilled \_\_\_\_\_ Other

Percentage of Work to be Done by Bidder's Employees (Based on Dollars Bid): 84%

Type(s) of Work to be Done by Bidder's Employees (Examples: Concrete Paving, Structural Concrete, Water Lines, Sanitary Sewer Lines, Storm Pipe, Storm Inlets, Excavation, Lime, Bridge Fencing, etc.)

Excavation, Embankment, Lime, Grading, Concrete Paving, Water Line, Sanitary Sewer Lines,

Storm Drain and Inlets, Bridge Structural Concrete and Beams, Retaining Walls, Flatwork

Access to Tools and Equipment: Percent Owned: 90% Percent Rented: 10%

Number of Years in Business as a Contractor on Above Types of Work: 133

Sub-Contractor Information (use additional sheets, if needed.):

Name: See 24-08-C - Breakdown Subcontractors Suppliers Phone: \_\_\_\_\_

Address: \_\_\_\_\_

Type of Work \_\_\_\_\_

List the most current completed projects like the type of work bid (use additional sheets, if necessary.)

Project: SH 31 Corsicana Relief Route

Owner/Agency: TxDOT

Year Built: 10/24/2016-1/25/2022 Contract Price: \$111,404,176

Equipment Used: Dozers, Loaders, Excavators, Compactors, Rollers, Motorgrader, Cranes, Bidwell, drill rigs, Water Trucks, Dump Trucks, Gomaco Concrete Paver

Contact Name: Brenan Honey Title: Area Engineer

Email: brenan.honey@txdot.gov Phone: 214.799.4547

Project: SH31 Widening CR 3344 to Hill County Line (Hubbard Bypass)

Owner/Agency: TxDOT

Year Built: 8/16/2017-1/14/2020 Contract Price: \$65,505,068

Equipment Used: Excavators, Dozers, Loaders, Articulating Offroad Trucks, Compactors, Rollers, Water Trucks, Motorgrader, Cranes, Drill Rigs, Bidwell, Concrete Pump Truck

Contact Name: Josh Voiles Title: Area Engineer

Email: josh.voiles@txdot.gov Phone: 254.582.5432

Project: San Pedro Creek Culture Park Phase 1.1 & 1.2

Owner/Agency: San Antonio River Authority

Year Built: 11/2/2016-7/6/2022 Contract Price: \$133,887,547

Equipment Used: Dozers, Excavators, Loaders, Skidsteer, Compactors, Telehandler, Bidwell, Hydraulic Cranes, Drill Rigs

Contact Name: Stephen Graham Title: Assistant General Manager

Email: sgraham@sariverauthority.org Phone: 210.227.1373

Sub-Contractor Information (use additional sheets, if needed.):

Name: See 24-08-C - Breakdown Subcontractors Suppliers Phone: \_\_\_\_\_

Address: \_\_\_\_\_

Type of Work \_\_\_\_\_

List the most current completed projects like the type of work bid (use additional sheets, if necessary.)

Project: SR 77 Gila River Bridge

Owner/Agency: Arizona DOT

Year Built: 9/5/2019 - 6/4/2021 Contract Price: \$12,667,749

Equipment Used: Dozers, Loaders, Excavators, Rollers, Water Truck, Motorgrader, Cranes, Bidwell, Concrete Pump Truck

Contact Name: Elaine Leavens-Cooke Title: SE District Consultant RE

Email: Eleavens-cooke.consultant@azdot.gov Phone: 480.271.4185

Project: I-10 CD Lanes (El Paso)

Owner/Agency: TxDOT

Year Built: 7/20/2015 - 10/31/2019 Contract Price: \$160,995,231

Equipment Used: Dozers, Loaders, Excavators, Motorgraders, Telehandlers, Cranes, Bidwell, Concrete Pump Truck, Dump Trucks, Rollers, Compactors

Contact Name: Mo Moabed, PE Title: Area Engineer

Email: mo.moabed@txdot.gov Phone: 915.757.5901

Project: Seeling Channel Improvements Phase 3

Owner/Agency: City of San Antonio

Year Built: 9/2/2020 - 3/11/2022 Contract Price: \$15,421,849

Equipment Used: Dozers, Excavators, Loaders, Skidsteer, Compactors, Telehandler, Bidwell, Hydraulic Cranes, Drill Rigs

Contact Name: Eric Salazar, PE Title: COSA Project Manager

Email: Eric.Salazar@sanantonio.gov Phone: 210.286.7729

**Trade References (attach additional sheets if needed):**

Company: Big Creek Construction Contact: Wade Miller

Address: 1617 N. Old Temple, Lorena, TX 76655

Email: wmiller@bigcreekconstruction.com Phone: 254.857.3200

Company: CMC Rebar Contact: Tully Williams

Address: 4846 Singleton Blvd, Dallas, TX 75212

Email: tully.williams@cmc.com Phone: 214.803.3946

Company: Martin Marietta Materials Contact: Eric Feliciano

Address: 1805 L Don Dodson, Bedford, TX 76021

Email: eric.felegiano@martinmarietta.com Phone: 972.647.3341

**Bank References (attach additional sheets if needed):**

Institution: JPMorgan Chase Commercial Banking Contact Name: Michael V. McCann

Address: 201 N. Central Ave., Floor 21 Phoenix, AZ 85004 Phone: 602.221.2830

Financial statements are not required as part of this bid package, however a balance sheet and income statement from the previous fiscal year shall be required from the apparent low bidder to be reviewed and approved by the Finance Director prior to contract award.

**Claims and Suits (If the answer to any of the questions is yes, please attach details):**

Has your organization ever failed to complete any work awarded to it?  Yes  No

Are there any judgments, claims, arbitration proceedings, or suits pending or outstanding against your organization or its officers?  Yes  No

Has your organization filed any lawsuits or requested arbitration regarding construction contracts within the last five years?  Yes  No

Within the last five (5) years, has any officer or principal of your organization ever been an officer or principal of another organization when it failed to complete a construction contract?  
 Yes  No



# SUNDT

## **Are there any judgments, claims, arbitration proceedings, or suits pending or outstanding against your organizations or its officers?**

Sundt Construction, Inc. is a national construction organization which is licensed to do business in approximately 25 different states. As such, it becomes involved in legal proceedings from time to time in the ordinary course of business, including claims made against or by the company. The majority of those matters involve liability, if any, which is covered by insurance. Sundt Construction, Inc. has never had a judgment entered against it in regards to the performance of its work but seeks diligently to resolve disputes in an amicable manner. The company does not anticipate that any pending suits will have, individually or in the aggregate, a material adverse effect on its financial position, results of operations, or liquidity. During the last five (5) years, the company notes the following:

### **Novato CA Real Estate, LLC v. Sundt Construction, Inc. (Filed 10/22/21, Marin County Superior Court)**

In October 2021, a project owner (Novato CA Real Estate) filed suit against Sundt in relation to a project to be completed in early 2022. The dispute alleges delays to the project, in addition to issues relating to water intrusion, all of which Sundt disputes. Sundt will be asserting a counterclaim against the Owner in the near future. The matter was amicably resolved in the middle of 2023.

### **Tower 180 Owner, LLC v. Sundt Construction, Inc. (Filed 8/17 /20, San Diego Superior Court)**

In August 2020, a project owner (Tower 180 Owner, LLC) filed suit against Sundt in relation to a project completed in early 2020. The dispute relates to the presence of unknown and undisclosed hazardous materials in the owner's building prior to the commencement of refurbishment work by Sundt. The owner agreed that arbitration is the dispute resolution process mandated by the prime contract and stayed the state court action. The parties have agreed to engage in a mediation in an attempt to resolve the issues.

### **The Regents of the University of California v. Sundt Construction, Inc. (Filed 2/15/18, Volo County Superior Court)**

In 2018, a project owner filed suit against Sundt in relation to a project completed in mid- 2014. Sundt worked cooperatively with the owner after concerns were first raised about possible water intrusion issues to identify the cause and extent of any such issues. After about a year of investigation, Sundt and the owner were able to reach an amicable settlement without resort to formal dispute resolution proceedings. The matter was resolved in 2019.

### **Pepperdine University v. Harley Ellis Deveraux, et al. (Filed 9/15/22, Los Angeles Superior Court)**

On September 15, 2022, an Owner joined Sundt in a lawsuit filed against the design professional in relation to Pepperdine University's Outer Precinct Seaver Residence Hall, a project completed in 2019 and which the owner is occupying and utilizing for its intended purpose. The dispute relates to alleged concerns regarding the installation of certain components at the project. Sundt is in the process of determining the nature of the allegations and intends to engage in mediation in an attempt to resolve the issues.

## **Has your organization filed any lawsuits or requested arbitration proceedings, or suits pending construction contracts within the last five years?**

During the last five (5) years, the company has only very rarely pursued litigation against other parties regarding its construction projects. See below:

### **Arbitration by Sundt against Owner on Hilton Canopy San Antonio**

In mid-2021, Sundt filed a demand for arbitration against a commercial developer in relation to a project completed in early 2021. The dispute relates to unpaid contract amounts and withheld retainage for a project that is completed, occupied and being used for its intended purpose. Arbitration is the dispute resolution process mandated by the prime contract. The parties amicably resolved this disputes in March 2022.

### **Sundt Construction, Inc. v. Dublin CA Real Estate, LLC, et al. (Filed 4/19/21, Alameda County Superior Court}**

In mid-2021, in compliance with state law, Sundt filed a lawsuit in order perfect its lien against a project. The lien (and the lawsuit) were necessary for Sundt to protect its right to payment of amounts sought in relation to a project completed in early 2021. The dispute relates to unpaid contract amounts for a project that is completed and being used for its intended purpose. The owner has also filed a cross-complaint against Sundt. Sundt is attempting to arrange a mediation or other non-litigation means of resolving the dispute. The parties resolved the matter amicably in August 2023.

### **Sundt Construction, Inc. v. Sunnyvale Hotels, LLC, et al. (Filed 1/14/21, Santa Clara County stayed and moved to arbitration}**

In early 2021, Sundt was compelled to file suit against a project owner in relation to a project which had been completed earlier that year and which the owner was occupying and utilizing for its intended purpose. The suit was filed as a necessary legal requirement in order for Sundt to protect its lien for progress payments which the Owner was withholding. The Owner agreed that arbitration is the dispute resolution process mandated by the prime contract and the parties have stayed the state court action. The matter resolved amicably in early 2022.

### **Sundt Construction, Inc. v. Northern California Officers Community, dba Paradise Valley Estates (Filed 11/15/22, Solano County Superior Court)**

In late 2022, in compliance with state law, Sundt filed a lawsuit in order to perfect its lien against a project. The lien (and the lawsuit) were necessary for Sundt to protect its right to payment of amounts sought in relation to a project completed in 2022. The state court action will be stayed as arbitration is the dispute resolution process mandated by the prime contract and Sundt has filed an arbitration demand. Sundt has been engaged with the Owner in discussion around resolving the dispute amicably, including through a mediation scheduled in early 2024.

# COMPETITIVE SEALED PROPOSAL

*November 15, 2023*

City of Fort Worth  
West 7th Street Bridge



# SUNDT

---

COMPETITIVE SEALED PROPOSAL / CITY OF LEWISVILLE / SUNDT CONSTRUCTION, INC.

---

*For Construction of Corporate Drive Segment 2 & 3, CSP #24-08-C*



# SUNDT

November 15, 2023

City of Lewisville  
Finance Administration - Purchasing Division  
151 W. Church St.  
Lewisville, Texas 75057

**RE: CSP #24-08-C, Corporate Drive Segment 2 & 3, Project No. G1122 & G1123**

Dear Proposal Evaluation Committee Members,

Sundt is committed to partnering with the City of Lewisville to complete Corporate Drive Segments 2 & 3, and we will relentlessly execute this project to the high standards for quality, safety, and schedule performance we have delivered on Windhaven Parkway. This will be accomplished by an on-site project team that have worked together on three roadway and bridge projects within the past five years including the City of Lewisville Windhaven Parkway project. Their experience is supplemented with a regional team that will provide additional expertise and resources to optimize innovation and execute the work in the most efficient manner possible.

Our key team members were selected because of their roadway and bridge experience and existing relationship with the City of Lewisville staff. They will support the City to build a successful project. This team is familiar with the City's processes and procedures and knows how to successfully complete roadway and bridge projects like Corporate Drive. Our project team carefully analyzed the plans and technical specifications while preparing this proposal. We look forward to working with the City of Lewisville and Halff to finalize several key aspects of our approach, including:

- Sequencing the work to allow 90 days for acquiring right-of-way and access agreements with no additional costs
- Minimizing impacts to recreational activities on the Elm Fork Trinity River while constructing the Trinity River Bridge in the winter months
- Protecting the environmentally sensitive areas identified in the US Army Corps of Engineers Individual Permit
- Minimizing impacts to the adjacent Waste Management and Camelot Landfill operations

**As a result, Sundt is confident in committing to final acceptance in 835 calendar days, 80 days ahead of the duration expected by the City of Lewisville.** In addition, should the City obtain access sooner than 90 days, final acceptance can be obtained up to 130 days ahead of the expected duration.

👍👍 *Sundt's #1 strength is their partnering. Successful companies like Sundt are focused more on solutions than on getting what they want. They operate out of a true team concept. Sundt is the 'best in class' in the industry.*

~ Brian Barth, District Engineer, TxDOT

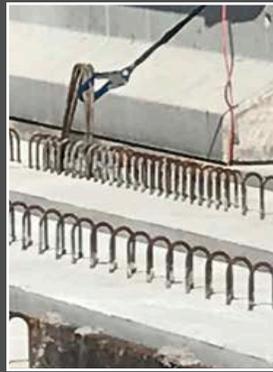
Sundt values public and workforce safety above all else. We were recognized by the Associated General Contractors of America as the safest Heavy Highway Contractor in America in 2006 and 2016.

We are excited to be considered for this important City of Lewisville infrastructure project. Should you have any questions during the review of our proposal, please do not hesitate to contact me.

Sincerely,

Jeff J. Williamson, Transportation Group President  
jjwilliams@sundt.com / 480.293-3263

City of Lewisville  
Windhaven Parkway



*Tab*

**A**

# COST PROPOSAL FORM



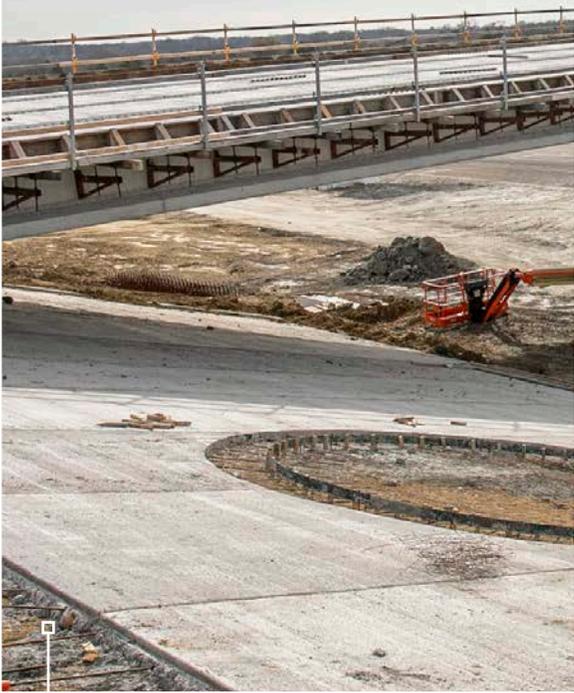
*Section A*

# COST PROPOSAL FORM

---

Sundt, in compliance with the CSP requirements, has uploaded the Cost Proposal Form separately, directly to Bonfire.

TxDOT SH 31 Widening CR 3344 to Hill  
County Line Hubbard Bypass



*Tab*  
**B**

**PROPOSER'S OTHER REQUIRED  
FORMS**



*Section B*

# PROPOSER'S OTHER REQUIRED FORMS

---

Sundt, in compliance with the CSP requirements, has uploaded all required forms directly to Bonfire.

Below is a listing of forms uploaded:

- Bid Bond
- Proposal Form
- Qualification Statement
- Additional Terms Form
- Acceptance of Evaluation Methodology
- Vendor Supplemental Information Form
- Subcontract Breakdown
- State Reciprocal - Non Collusion
- Exceptions
- Recent Financial Statement

COSA Hausman Road Improvements



*Tab*

**C**

# QUALIFICATIONS AND RELEVANT PROJECT EXPERIENCE OF PROPOSER



# QUALIFICATIONS AND RELEVANT PROJECT EXPERIENCE OF PROPOSER

## PROPOSER’S BUSINESS ORGANIZATION

Sundt is a 100 percent employee-owned company headquartered in Tempe, Arizona. Our regional office in Irving, Texas will be responsible for executing all aspects of this project. A corporate organization chart including the names and titles of key executives operating our business is shown on the following page.

Sundt is a different kind of construction company. We’re modern and innovative, but with very deep roots. Our principles of honesty and integrity originated with the founder, Mauritz Sundt in 1890. He built his business with hard work and a dedication to doing things the right way. By the 1920s, Mauritz’s son, Thoralf, was designing buildings for the company, while son John opened and operated an office in Tucson, Arizona, calling it M.M. Sundt Construction Co. In 2000, Sundt became a 100% Employee-Owned Company.

Today, Sundt employs 2,883 people, with offices in Texas, Arizona, California, Utah, Washington, North Carolina, and Florida. With annual construction revenues exceeding \$1.6 billion, Sundt is the 62nd largest contractor in the US according to the Engineering News-Record. Sundt provides a wide range of services to its clients, including Construction Management at Risk, General Contracting, Design-Build, and Build-to-Suit.

With a large and diverse force of skilled craft professionals, Sundt self-performs critical scopes of work in the construction process, including structural concrete, flatwork, concrete paving, structural steel, excavation and grading, underground utilities, drainage systems, and embankment. By performing these scopes of work ourselves, Sundt effectively manages critical-path schedule, safety and quality control for the entire project—all advantages that create greater value for our clients. We are able to quickly identify and solve project challenges, implement changes, and reduce or eliminate subcontractor delays. Our craft professionals take pride in their work, which leads to higher quality of construction and a better end product.

### Corporate Name and Address

Sundt Construction, Inc.  
2620 S 55th Street  
Tempe, AZ, 85282

### Local Branch Office

8445 Freeport Pkwy, Suite 240  
Irving, TX 75063

### Company Name History

Business Names	From	To
M.M. Sundt Builder	1890	1946
M.M. Sundt Construction Company	1946	1991
Sundt Corp.	1978	1998
Sundt Construction, Inc.	1998	Present

25

Build America Awards

2,880+

Skilled Construction Staff

300+

Craft Workers in Texas

44

Transportation Projects Completed in the Past 10 Years

5M+

Self-Performed Manhours Per Year

0.60

Average Experience Modifier Rate

## SECTION C / QUALIFICATIONS AND RELEVANT PROJECT EXPERIENCE OF PROPOSER



### SAFETY

Backed by our industry-leading average EMR of 0.60 over the last five years, we ensure everyone goes home safely.



### QUALITY

Our verified quality control process delivers quality throughout the project life cycle.



### SCHEDULING

Scheduling experts create detailed schedules to make certain the project stays on track.



### VIRTUAL DESIGN CONSTRUCTION

The most up-to-date technology gives you the most realistic previews of your project.



### SUSTAINABILITY & RESILIENCY

Our efficient use of materials creates less waste and more long-term function.



### DIVERSITY & INCLUSION

We actively seek to incorporate everyone.



### WORKFORCE DEVELOPMENT

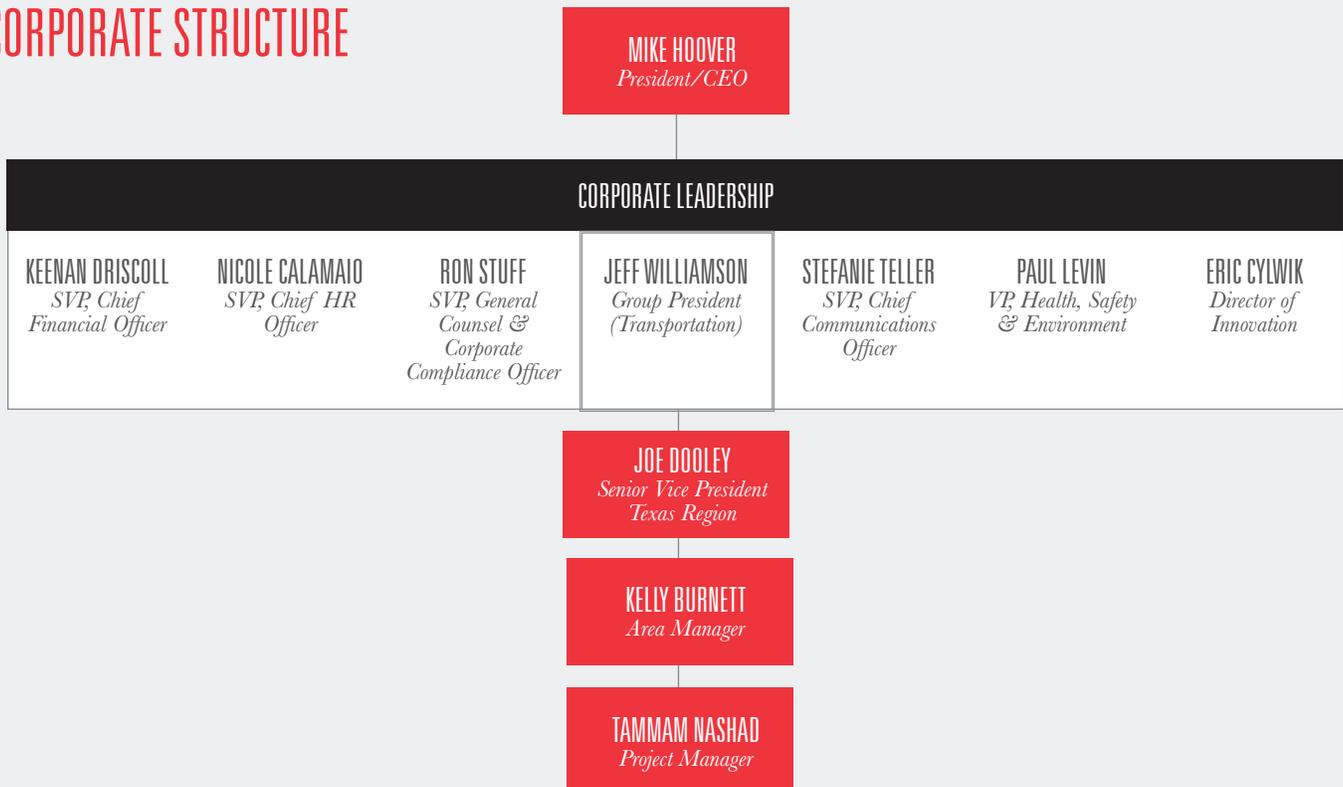
We leave a trained workforce when the project is complete.



### SUNDT FOUNDATION

As good stewards, we provide scholarships to individuals and donations to charity organizations.

## CORPORATE STRUCTURE



## RELEVANT EXPERIENCE

Sundt's experience in North Texas runs deep. Having built two of the most recognizable features of the Dallas and Fort Worth skylines, Reunion Tower and City of Fort Worth West 7th Street Bridge, Sundt has been integral in the history of these landmark projects. In 1978, Sundt built Reunion Tower, in just 68 days: the project won the AGC Alliant Build America Award and still stands as an iconic part of the Dallas skyline. The City of Fort Worth West 7th Street Bridge, completed in 2013, won multiple awards, including the Build America Award. Our high-performing Texas region currently holds four of Texas & Louisiana's 2022 Best Project Awards from Engineering News-Record and has completed more than \$3 Billion of project work throughout the state during the past 50 years.

As Sundt is currently completing the City of Lewisville's Windhaven Parkway Project, the City's management team will benefit from a team led by the same Key Personnel and consistency in the project management documents and work plans that will be submitted on time, well organized, and in a familiar format. Based on Sundt's strict corporate standards, they will have already been approved and implemented on our other City project and will reflect any continuous improvement initiatives requested by the City.

The five projects on the following pages showcase our experience with public sector roadway and bridge projects of similar scope and size to the Corporate Drive Segment 2 & 3 project.

“*Sundt was definitely committed to the success of this project by how focused they were in developing a true partnership. The evidence of the success of this project is demonstrated by the awards we have already won as well as the awards we will continue to win in the future. This was a high profile project for the City and it was extremely important.*

~ Brian Barth, District Engineer, TxDOT

## CASE STUDY



### *City of Fort Worth West 7th Street Bridge*

Sundt employed a number of innovative approaches to minimize traffic closures during the 23-month project. Chief among them was the decision to construct the 12 concrete arches off site (with Sundt's own concrete construction crews) and then place them in pairs along either side of the old structure. This approach allowed the bridge to remain open to traffic for the four months that it took to make and cure each arch. Once all of the arches were in place, the project team closed and demolished the old bridge and constructed the new one in its footprint. We removed and **replaced the existing bridge in four months, 30 days ahead of our contractual obligation.**

## AWARD WINNER

10 total awards, including:

- ✓ AGC Alliant Build America Award - Highway and Transportation Division
- ✓ TxDOT Journey Towards Excellence: Construction Award
- ✓ ENR Regional Best Projects - Highway and Bridge Division



### *Project Information*

**Client:**

City of Lewisville

**Supervisors:**

Kelly Burnett, Area Manager

Tammam Nashad, Project Manager

Noel Estrada, Superintendent

Dale Kerr, Project Engineer

**Equipment Used:**

Drill Rigs, Excavators, Dozers, Loaders, Cranes, Concrete Pump Trucks, Concrete Paver, Motorgrader

**Original Contract Amount:**

\$16,571,806

**Final Contract Amount:**

\$16,571,806 (forecasted)

**Start Date:** 5.11.2022

**Completion Date:** 6.5.2024

**Original Duration:** 915 days

**Final Duration:** 756 days (forecasted)

**Delivery Method:**

Design-Bid-Build

**Owner's Representative:**

David Salmon, PE, CFM, City Engineer, 972.219.3491

dsalmon@cityoflewisville.com

## DESCRIPTION

Sundt constructed approximately 1,500-ft of Windhaven Parkway, a four-lane divided roadway, from Cookie Lane to Brown Knight Lane. The work included a new railroad bridge for KCS Railroad. We installed approximately 2,700-ft of temporary shoofly track to construct the new bridge structure and removed the shoofly track once the bridge was complete. Scope of work included constructing the new roadway underpass, permanent track and retaining walls (cast-in-place, drilled shaft, and mechanical stabilized earth), removing the shoofly track, constructing concrete pavement, and installing waterline utilities. Sundt constructed a new 200-hp pump station to remove storm water from the underpass roadway during storm events. In addition, the project included mass excavation, minor traffic control, landscaping, and signing and striping.

This project is **tracking to complete five months ahead of schedule**, and it has ZERO Non-Conformance Reports and ZERO Lost Time Incidents.

### RELEVANT SCOPE TO CORPORATE DRIVE:



**Roadway Construction:** Roadway work included a new alignment four-lane divided concrete pavement roadway section with curb and gutters and new six-foot sidewalks constructed to City of Lewisville details.



**Bridge Construction:** Sundt constructed a new two-span, 100-ft-long railroad bridge featuring 60-in drilled shafts and columns, caps, TX 54 beams, and 4,850-sf of bridge deck constructed.



**Utility Construction:** Sundt relocated existing water mains, including tie-ins and fire hydrants, to construct the new drainage system.



**Drainage Construction:** Sundt installed gravity and force main drainage including 18-in, 24-in, 27-in and 30-in reinforced concrete drainage pipe and associated inlets, a new 8x6-ft precast concrete box culvert, 16-in and 30-in force main drainage pipe, and a pump station with submersible pumps and associated piping to remove the storm water from the underpass.



**MSE Retaining Wall Construction:** Sundt constructed 8,732-sf of MSE retaining walls, along with drilled shaft and cast-in-place walls.



### *Project Information*

**Client:**

City of San Antonio

**Supervisors:**

Chris Leintz, Project Manager  
Cesar Salazar, Superintendent

**Equipment Used:**

Drill Rigs, Excavators, Dozers,  
Loaders, Cranes, Concrete  
Pump Trucks, Bidwell, Manlifts,  
Motorgrader

**Original Contract Amount:**

\$71,588,000

**Final Contract Amount:**

\$66,926,010

**Start Date:** 12.2.2013

**Completion Date:** 1.26.2017

**Original Duration:** 759 days

**Final Duration:** 1151 days\*

\*Additional time due to a delay  
in ROW acquisition by TxDOT.  
Project completed on time per  
the revised duration.

**Delivery Method:**

Progressive Design-Build

**Owner's Representative:**

Razi Hosseini, Assistant Director  
210.286.2454  
Razi.Hosseini@sanantonio.gov

## DESCRIPTION

Hausman Road was the City of San Antonio's first progressive design-build transportation project and the largest street project in its history. Hausman Road was a two-lane roadway that connected two major highways, Loop 1604 and Interstate 10. Sundt reconstructed the roadway to four lanes divided, constructed five roadway bridges across creeks, upgraded utilities, and added center medians and curbs. We also built sidewalks, driveway approaches, and a shared-use pedestrian and bike path. Work included upgrading traffic signals throughout the corridor, and designing and constructing utilities (water, sewer, gas, electric, and telecom). Due to ROW delays, we re-sequenced the project to minimize impacts to the project schedule.

### RELEVANT SCOPE TO CORPORATE DRIVE:



**Roadway Construction:** Sundt demolished and reconstructed 3.3-mi of arterial roadway, which included six signalized intersections. New pavement included 34,000-sy of concrete paving, 30,000-sy of asphalt paving, and moisture-treated subgrade.



**Bridge Construction:** The project included construction of five multi-span bridge structures over creeks. The substructure and superstructure were constructed to TxDOT details similar to those on Corporate Drive.



**Utility Construction:** Sundt coordinated with the utility companies to move their electrical, gas, and third-party fiber optic lines ahead of construction, and relocated water and sewer lines. The scope included building more than 6,300-lf of San Antonio Water Services (SAWS) water line (8-in PVC or 16- to 30-in C-301 water main), and installing 7,600-lf of sewer line (6- to 18-in PVC).



**Drainage Construction:** The project included construction of drainage systems and water quality systems. Sundt installed over 1,000-lf of precast box culverts and 1,800-lf of 12- to 36-in RCP drainage pipe.

“*The best contractors are those who are responsive and are willing to take risks upon themselves to serve the greater good. This builds trust and Sundt is included among those best contractors.*”

~ Richard Grochowski, Project Manager, City of San Antonio



Hubbard, TX

# SH 31 WIDENING (HUBBARD BYPASS)

## Project Information

### Client:

TxDOT

### Supervisors:

Kelly Burnett, Area Manager  
Chris Leintz, Project Manager  
Pascacio Chavez,  
Superintendent

### Equipment Used:

Drill Rigs, Excavators, Dozers,  
Loaders, Cranes, Concrete  
Pump Trucks, Bidwell, Manlifts,  
Motorgrader, Off-Road  
Articulating Trucks

### Original Contract Amount:

\$63,963,884

### Final Contract Amount:

\$65,505,068\*

**Start Date:** 8.16.2017

**Completion Date:** 1.14.2020

**Original Duration:** 881 days

**Final Duration:** 881 days

### Delivery Method:

Design-Bid-Build

### Owner's Representative:

Josh Voiles, Asst. Area Manager  
254.582.5432  
josh.voiles@txdot.gov

*\*As with typical TxDOT projects, the majority of contract cost variation was contributed to multiple over/under on unit price items.*

## DESCRIPTION

This \$65.5-million project for TxDOT improved almost 10-mi of Texas State Highway 31 in Hill Country, Texas. Sundt created a 5.5-mile-long realignment of the highway around of the town of Hubbard; work included widening and reconstructing the existing two-lane rural highway to four lanes with grade-separated interchanges and construction 12 new bridges, 32 concrete box culverts, and over 2.5 million-cy of embankment.

The new bypass helps to reduce congestion by routing through-traffic around the southern end of Hubbard, rather than through town. New grade separations at five existing intersections improve safety by eliminating crossings with the highway, and improved drainage crossings help to mitigate flooding.

Sundt was able to deliver the project on time despite the record rains (more than double the rainfall in a typical year for Hill County) encountered throughout the project.

### RELEVANT SCOPE TO CORPORATE DRIVE:



**Roadway Reconstruction:** The project was a combination of constructing 5.5 miles of new alignment roadway and reconstructing 4.5 miles of existing two-lane roadway to four lanes divided. Sundt embanked 2.5 million-cy of earthwork, placed 147,099-cy of flex base, and 197,420-tn of asphalt.



**Bridge Construction:** Sundt constructed 12 new multi-span bridges using standard TxDOT details similar to the Corporate Drive project. Overall, the project team constructed 121,524-sf of new bridge deck.



**Utility Construction:** The project included installing 20,552-lf of waterline ranging in size from 2- to 12-in and 1,049-lf of sanitary sewer lines ranging in size from 8- to 12-in.



**Drainage Construction:** Sundt installed 7,349-lf of box culverts ranging in size from 3x2-ft to 10x7-ft, and 11,454-lf of RC pipe ranging in size from 18- to 36-in, along with the inlets and wingwalls.



### *Project Information*

**Client:**

TxDOT

**Supervisors:**

Kelly Burnett, Area Manager  
Josh Bunting, Project Manager  
Pascacio Chavez,  
Superintendent

**Equipment Used:**

Drill Rigs, Excavators, Dozers,  
Loaders, Compactors, Cranes,  
Concrete Pump Trucks, Bidwell,  
Manlifts, Motorgrader, Off-Road  
Articulating Trucks

**Original Contract Amount:**

\$105,980,139

**Final Contract Amount:**

\$111,404,176\*

**Start Date:** 10.24.2016

**Completion Date:** 1.25.2022

**Original Duration:** 1,803 days

**Final Duration:** 1,919 days\*\*

**Delivery Method:**

Design-Bid-Build

**Owner's Representative:**

Brenan Honey  
214.799.4547  
brenan.honey@txdot.gov

*\*As with typical TxDOT projects,  
the majority of contract cost  
variation was contributed to  
multiple over/under on unit price  
items.*

*\*\*Additional duration due to  
delays in oil field mitigation. The  
project completed ahead of the  
revised duration.*

## DESCRIPTION

This project for the TxDOT Dallas District included the full reconstruction of two miles of I-45 and new alignment construction of 12 miles of SH 31 to bypass the town of Corsicana.

Sundt reconstructed I-45 under continuous traffic to create a new grade-separated interchange with the new SH 31 roadway. The project includes 10 new bridges, retaining walls, earthwork, concrete paving, storm drain, and phased construction. With the project crossing eight separate FEMA flood zones, the new constructed crossings help mitigate flooding issues in the area.

Construction of SH 31 was new alignment, greenfield construction similar to Corporate Drive. Two of the 10 bridge structures were constructed over active creeks.

### RELEVANT SCOPE TO CORPORATE DRIVE:



**Roadway Construction:** The project included 1.5 million CY of embankment, 698,929-cy of flex base, 238,434-ton HMA and 119,778-sy of 10.5- to 13-in concrete pavement. Scope included installing new signing, striping and lighting along the roadway.



**Bridge Construction:** Project included 10 multi-span bridges constructed using standard TxDOT details similar to the Corporate Drive project. Quantities include: 17,484-lf of 18-in, 36-in and 42-in drilled shafts, 4,182-cy of substructure concrete, 471,971-sf of bridge deck, and 57,858-lf of TX-46, 54 and 70 girders.



**Drainage Construction:** Sundt installed 6,272-lf of box culverts ranging in size from 3x2-ft to 10x6-ft and 9,025-lf of RC pipe ranging in size from 18-in to 48-in, along with the inlets and wingwalls



**MSE Retaining Wall Construction:** Sundt constructed five MSE retaining walls totaling 38,608-sf and ranging from 13- to 23.5-ft tall.



LaRue, TX

# US 175 WIDENING

## Project Information

### Client:

TxDOT

### Supervisors:

Chris Leintz, Project Manager  
Marco Orona, Superintendent

### Equipment Used:

Drill Rigs, Excavators, Dozers,  
Loaders, Compactors, Cranes,  
Concrete Pump Trucks, Bidwell,  
Manlifts, Motorgrader, Scrapers

### Original Contract Amount:

\$37,849,155

### Final Contract Amount:

\$38,675,125\*

**Start Date:** 7.16.2015

**Completion Date:** 07.24.2017

**Original Duration:** 739 days

**Final Duration:** 658 days

### Delivery Method:

Design-Bid-Build

### Owner's Representative:

Kim Soucek (Formerly TxDOT)  
979.446.4440  
ksoucek@consoreng.com

*\*As with typical TxDOT projects, the majority of contract cost variation was contributed to multiple over/under on unit price items. In addition, TxDOT approved a \$50,243 change order for unforeseen conditions.*

## DESCRIPTION

Sundt widened 4.5 miles of US 175 southeast of Dallas from two lanes undivided to four lanes divided. Work included excavation, embankment, grading, sub-base, base, HMAC, precast and cast-in-place drainage structures, four bridges, and new signage.

The project team was able to overcome record high rainfall for two consecutive years with innovative de-watering methods and customized forming systems that allowed the project to complete ahead of schedule by 81 days.

### RELEVANT SCOPE TO CORPORATE DRIVE:



**Roadway Construction:** Sundt embanked 1.4 million-cy, lime treated 101,607-sy, and placed 76,788-cy of flexbase, and 69,582-tn of HMAC. We installed new signing, striping and lighting, and along the roadway.



**Bridge Construction:** Sundt crews constructed four bridges using TxDOT details similar to the Corporate Drive project. Quantities included 2,524-lf of 36-in drilled shaft, 460-cy of substructure concrete, 44,000-sf of bridge deck, and 5,467-lf of TX-40 and 54 girders.



**Drainage Construction:** Sundt reconstructed or upgraded 17 reinforced box culvert crossings during construction, maintaining drainage at all times. Drainage improvements include: 11,453-lf RCBC up to 10x9-ft; 12,598-lf of 18- to 24-in RC pipe; 34,650-lf of 36-in RC pipe; 61,280-lf of 48-in RC pipe. Sundt constructed the headwalls, wingalls and inlets for the drainage structures.

“ Thank you for bringing the materials, people, equipment, quality mindset, and partnering attitude needed to do the job safely and steadily execute your plan.

~ Kim Soucek, PE, Athens Area Engineer, TxDOT



*Tab*  
**D**

# EXPERIENCE & QUALIFICATIONS OF PROPOSED PROJECT PERSONNEL



Section D

# EXPERIENCE & QUALIFICATIONS OF PROPOSED PROJECT PERSONNEL

Sundt emphasizes consistent collaboration in all project phases. Combining critical City of Lewisville and Halfp knowledge with the pre-bid work done by our team is the key to delivering a high-quality project, which means:

- Providing exceptional safety to the public and our crews
- Minimizing inconveniences to the public and adjacent landowners by working with the City of Lewisville to consider their needs when we plan work, and providing timely and accurate progress updates during construction
- Providing cost and schedule certainty in our bid, and following up with on-time and on-budget execution

## PEACE OF MIND

The Windhaven Pkwy project key personnel will be your team on this project bringing you the peace of mind of a smooth transition from one City project to the next.

## TEAM QUALIFICATIONS

We recognize the need to provide continuity of personnel through each phase of the project to assure high levels of productivity and quality. To assist in maintaining project goals, our project management structure includes several teams that will work both independently and in conjunction with each other with a combination of on-site project management and operational support from our Texas regional office. This structure is depicted on the project organization chart on page 14.

We carefully selected individuals from our organization who truly appreciate the opportunity to participate with owners and designers in the planning and execution of the construction project. Project Manager Tammam Nashad and the rest of the project team have access to regional and national resources from the Operational Support team.

### On-site Project Team

#### Tammam Nashad - Project Manager

Tammam will have overall responsibility for all aspects of the Corporate Drive Segment 2 & 3 project and will be the primary point of contact for the City. He has worked on a wide range of heavy civil infrastructure projects, from traditional bid-build projects to alternate delivery mega-projects.

Tammam's experience similar to the Corporate Drive Segment 2 & 3 project includes the Lake Ralph Hall Infrastructure Relocation, where he worked with Noel Estrada and Servando Elizalde. The project scope included construction of an elevated complex bridge structure over the Sulphur River, similar to the bridge structure over the Elm Fork Trinity River for Corporate Drive. Tammam has a verifiable track record of efficient project start-up, managing budgets and schedules, coordinating quality control, safely executing work, and providing accurate and timely information to owners and project teams. On the Windhaven project, working with Noel and Dale, Tammam has proven to be a strong leader, as the project is tracking to complete five months ahead of schedule and hasn't had any quality non-conformance reports (NCRs) or recordable incidents.

#### Noel Estrada - Superintendent

Noel will be 100% dedicated to the Corporate Drive Segment 2 & 3 project and will manage all production crews and subcontractors. In addition to his supervisory and training skills on roadway and bridge projects, Noel has 20 years of construction experience, more than 15 years of management experience.

Noel has recent experience working on roadway and bridge projects and has been working in North Texas with local craft professionals and subcontractors for the last 12 years. Noel supervises all phases of a construction project from initial planning to completion and creates safe and efficient teams.

#### Servando Elizalde - Safety Manager

Servando has 35 years of experience in safety planning and implementation, working on a variety of projects with stringent health and security requirements. He ensures compliance with the project safety plan,

## SECTION D / EXPERIENCE & QUALIFICATIONS OF PROPOSED PROJECT PERSONNEL

including pre-activity work plans, communication of site-specific hazards, inspections, and auditing. Servando has experience with similar scopes of work including earthwork, structural concrete, drainage, utilities, setting bridge beams and concrete paving. He will conduct new hire and subcontractor orientations, OSHA 30 training, and manage the accident prevention plans per project specifications.

### **Dale Kerr - Project Engineer/Quality Manager**

Dale will bring his recent experience as the Senior Project Engineer on the Windhaven Parkway Project for the City of Lewisville to this project. As Sundt's Project Engineer, he will also serve the role of Quality Control Manager. In this role, Dale will implement a successful three-phase quality program to ensure compliance to the Contract documents. He will provide guidance to the superintendents for construction documents, subcontractors, and purchase orders. He responds to questions about drawings, specifications, and shop drawings, ensuring comments are properly documented.

Dale has experience on major roadway infrastructure projects featuring similar scopes of work as Corporate Drive including drainage, bridge structures, earthwork, grading, concrete paving, illumination, signing and striping.

### **Operational Support Team**

#### **Kelly Burnett - Area Manager**

Kelly has over 30 years of industry experience and is responsible for supporting and supervising project execution through the designated project manager on big-picture project elements.

Kelly has local knowledge of the DFW-area market and has built strong relationships with the subcontractors, vendors and suppliers. He will ensure the Corporate Drive Segment 2 & 3 project receives access to any resources necessary for success and will meet with City representatives at least quarterly throughout the project duration to gauge your satisfaction. He is tasked with ensuring the onsite project team has the tools it needs to do the job right the first time.

#### **Omar Percy - Group Health and Safety Manager**

Omar will provide support to Servando and the team to ensure project safety. He monitors projects for health and safety and provides technical assistance to ensure that Sundt's loss control practices are being followed. Omar maintains current knowledge of legal and contractual requirements affecting safety, health,

and welfare and recommends procedures to improve existing safety methods.

#### **Amy Yount, PE - Group Quality Manager**

Amy has 15 years of experience in the construction industry. She leads the Sundt quality committee, which consists of quality leaders from all business groups to ensure alignment across the company for quality-related initiatives. She was also responsible for expanding the Sundt corporate quality program to comply with ISO 9001:2105 requirements. Amy will be a resource for Tammam and Dale to develop and manage the project Quality Management Plan including all start-up, pre-activity meetings, and pre-placement checklists.

## SUBCONTRACTORS AND SUPPLIERS

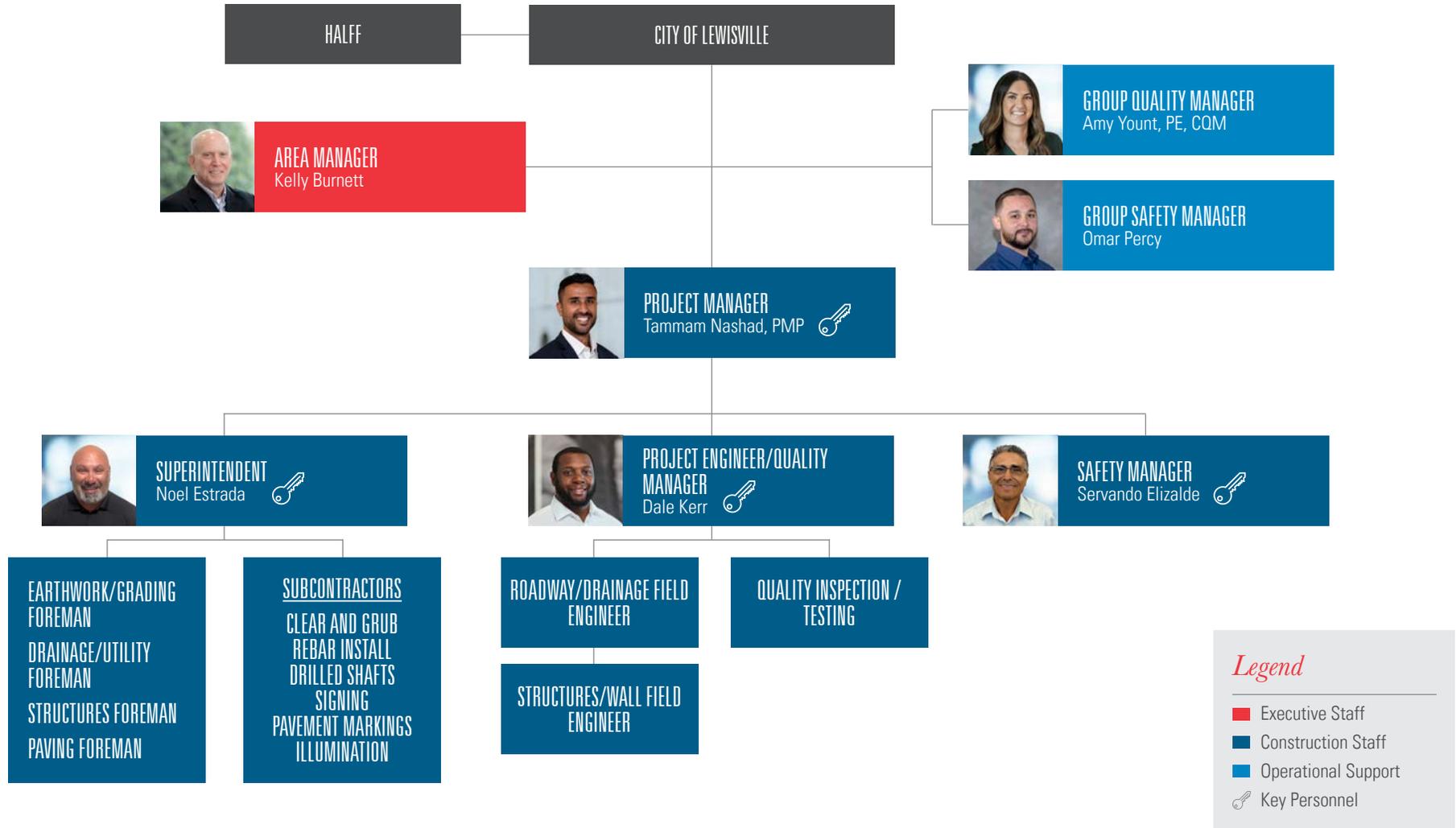
Sundt has selected subcontractors on a best-value basis after evaluating factors such as past performance related to safety, quality, and schedule, along with small business status, client references, financial status, proposed staffing, experience and current workload. Subcontracted scopes of work include clearing and grubbing, rebar furnishing and installation, fencing, hauling, signing, pavement markings, illumination, and laboratory services for quality testing.

Each subcontractor will, by terms of its subcontract, be responsible for complying with all aspects of the project quality and safety plans. Subcontractors will be required to attend a project orientation prior to being allowed on the project site or beginning work. They are also required to attend daily work plan meetings to review and acknowledge quality standards, hold points, testing, and inspection requirements prior to executing any scope of work.

Sundt will be responsible for assuring that subcontractors adhere to all aspects of the contract. We will monitor subcontractor activities to promote the active integration of quality work practices into their operations. Project Superintendent Noel Estrada will conduct scheduled and periodic unannounced inspections and record audits to ensure that provisions of the contract are administered. We will also provide special consideration and assistance to small businesses, including bonding assistance, adjusting contract terms, paying for materials on hand, etc., in order to engage more local and small business participation.

## ORGANIZATION CHART

We carefully selected individuals from our organization who truly appreciate the opportunity to participate with owners and designers in the planning and execution of the construction project. As shown on the following organization chart, Sundt's on-site project manager, Tammam Nashad, is responsible for the successful management of Sundt's operations with safety, quality and production.





## TAMMAM NASHAD, PMP

*Project Manager*

### *Education / Certifications*

MS, Transportation Systems, *University of Central Florida*

BS, Civil Engineering, *University of Benghazi*

OSHA 10 and 30-Hour  
First Aid and CPR Training

### *11 Years of Experience*

Tammam has significant experience on alternative delivery roadway and bridge projects, including several in the DFW metroplex. He has been involved in preparing conformed estimates, value engineering research, and constructability analysis during preconstruction. During construction, Tammam has coordinated work plans and resource allocation for roadways, structures, drainage, and maintenance of traffic crews.

### *Relevant Experience*

#### **City of Lewisville | Windhaven Parkway, Project Manager**

Tammam is responsible for all self-performed and subcontracted work. He oversees manpower and equipment resource allocation, stakeholder coordination, schedule updates, and ensures all work was done in accordance with project quality specifications. The project consists of construction of approximately 1,500-ft of four-lane divided roadway, a 100-ft long bridge for KCS Railroad, approximately 2,700-ft of temporary shoofly track to enable continuous freight operations during construction, a new roadway underpass, retaining walls, and installation of permanent track and associated structures and sidewalks. Utility work includes storm drains, a pump station, force mains, gravity drains, and water line relocations. **He is working collaboratively with Noel and Dale on this project.**

#### **Upper Trinity River Water | Lake Ralph Hall Infrastructure Relocation, Sr. Project Engineer\***

The project includes a mile-long bridge that will run over Lake Ralph Hall, a new reservoir that will provide approximately 35-million-gallons of water per day to North Texas communities. The bridge design includes paths for bicyclists and pedestrians. Nearly 372,000-cy of embankment will be utilized on this sustainable project. Tammam helped coordinate a new delivery milestone for the structures package design and prepare cost estimates for phased GMPs as early work packages that began drilled shaft installation, early procurement of precast girders, precast deck panels, and other commodities and early mobilization. This reduced the overall project completion time by three months. **He worked with Noel and Servando on this project.**

#### **FDOT I-4 Ultimate, Project Engineer\***

This project reconstructed and expanded 21 miles of tollway, including 140 bridges (13 widened, 74 replaced, and 53 newly constructed). Tammam helped to coordinate short-term lane closures. He played a pivotal role in the comprehensive coordination of construction operations for 17 new bridges, which included the notable erection of a 184-ft pedestrian steel bridge, demonstrating his adeptness at managing complex logistical challenges. He was instrumental in the planning and execution of extensive drainage system installations, surpassing 40,000-lf, ensuring the projects advanced without compromising existing infrastructure functionality. **He worked with Dale on this project.**

\*Denotes project prior to joining Sundt.



# NOEL ESTRADA

*Superintendent*

## *Education / Certifications*

*OSHA 30-Hour Training  
Traffic Control Training  
Confined Space Training  
First Aid and CPR Training  
Heavy Equipment Training*

## *20 Years of Experience*

Throughout Noel’s 20+ years of general construction experience and 15+ years of heavy civil roadway and bridge structures experience, he has proven his ability to complete projects consistently on time. He has an excellent ability to communicate and teach concepts and strategies throughout an organization. Noel’s experience also extends to effectively collaborating and coordinating with regulatory agencies and stakeholders and assisting with three-week look-ahead scheduling for owner meetings.

## *Relevant Experience*

### **City of Lewisville | Windhaven Parkway, Senior Project Superintendent**

Noel is responsible for for the management of all field work and ensuring the safety of all workers. The project consists of construction of approximately 1,500-ft of four-lane divided roadway, a 100-ft long bridge for KCS Railroad, approximately 2700-ft of temporary shoofly track to enable continuous freight operations during construction, a new roadway underpass, retaining walls, and installation of permanent track and associated structures and sidewalks. Utility work includes storm drains, a pump station, force mains, gravity drains, and water line relocations. ***He is working collaboratively with Tammam and Dale to ensure the project is successful.***

### **Upper Trinity River Water | Lake Ralph Hall Infrastructure Relocation, General Superintendent\***

The project includes a mile-long bridge that will run over Lake Ralph Hall, a new reservoir that will provide approximately 35 million gallons of water per day to North Texas communities. The bridge design includes paths for bicyclists and pedestrians. Nearly 372,000-cy of embankment will be utilized on this sustainable project. That’s enough to fill a 70-foot tall football field. Put together, all the beams on project weigh nearly 40,000,000 pounds. The tallest column on this project is 100-lf. This is the first TxDOT project to utilize the innovative progressive design-build delivery method. Noel oversaw the construction of infrastructure substructures, superstructures, earthwork and roads. ***He worked with Tammam and Servando on this project.***

### **City of Denton | Hinkle Drive Rehabilitation, General Superintendent\***

Noel assisted the project manager in coordinating self-performance crews and materials, coordinated scheduling with subcontractors, and supported the subcontractors with manpower and materials. He attended owner’s meetings and helped with the look-ahead schedule. Noel was the compliance champion, ensuring crews were not only following the city’s and engineer’s specifications, but that all parties were doing so safely. This project consisted of new underground utilities and new pavement from curb and gutter to new driveways.

\*Denotes project prior to joining Sundt.



# SERVANDO ELIZALDE

*Safety Manager*

Servando brings over 35 years of construction experience, with 22 years dedicated to safety. He began his career working in the field and has received numerous promotions due to his exceptional attention to detail and work ethic. Servando will ensure every aspect of the project is safe, provide assistance and support to site personnel concerning safety matters, and investigate and monitor accidents, incidents, and claims.

## *Relevant Experience*

### **City of Denton | Bonnie Brae Phase 3, Safety Manager**

This project is reconstructing two miles of Bonnie Brae Street; work includes removals, grading, retaining walls, 68,000-sy of concrete paving, 8,270-sy of asphalt paving, 16,500-sy of concrete shared-use paths and sidewalk, 9,900-ft of concrete storm pipe, 47 curb inlets, 4,700-ft of 12-in water line, 5,800-ft of 21- and 24-in sanitary sewer line, and 22 sanitary sewer manholes, plus signal lights, contraflow signals, street lights, pavement markings, and signage. Servando provides oversight for all project safety efforts, reviewed project safety plans, coordinated traffic control plans, walked the site monthly, and worked with the entire project team to plan and execute project safety.

### **Upper Trinity River Water | Lake Ralph Hall Infrastructure Relocation, Safety Manager\***

Servando provided oversight for all project safety efforts, reviewed project safety plans, coordinated traffic control plans, walked the site monthly, and worked with the entire project team to plan and execute project safety. The project includes a mile-long bridge that will run over Lake Ralph Hall, a new reservoir that will provide approximately 35 million gallons of water per day to North Texas communities. The bridge design includes paths for bicyclists and pedestrians. Nearly 372,000-cy of embankment will be utilized on this sustainable project. That's enough to fill a 70-foot tall football field. Put together, all the beams on project weigh nearly 40,000,000 pounds. The tallest column on this project is 100-lf. This is the first TxDOT project to utilize the innovative progressive design-build delivery method. *He worked with Tammam and Noel on this project.*

### **TxDOT | I-820 East Loop Project, Safety Manager\***

As part of the "Texas Clear Lanes Initiative," a plan put forth by the governor to ease traffic along well-known chokepoints, this multi-phase project will reconstruct Interstate 820 along a three-mile stretch within the city limits of Fort Worth, Hurst, and Richland Hills. Servando provided oversight for all project safety efforts, reviewed project safety plans, coordinated traffic control plans, walked the site monthly, and worked with the entire project team to plan and execute project safety. *He worked with Dale on this project.*

\*Denotes project prior to joining Sundt.

## *Education / Certifications*

OSHA 10- and 30-Hour  
First Aid and CPR Training  
Confined Space Training

## *35 Years of Experience*



## DALE KERR

*Project Engineer/Quality Manager*

Dale has seven years of field experience across large construction projects, including bridge work. He provides technical assistance to foremen, supervisors, and subcontractors, and facilitates RFI resolution with design engineers. He also monitors project daily progress reports, coordinates roadway material deliveries and manages roadway and earthwork finishing crews.

### *Relevant Experience*

#### **City of Lewisville | Windhaven Parkway, Senior Project Engineer**

**Working with Tammam and Noel**, Dale plays an integral part in project success. He provides oversight to the project team to ensure trade management is effective and consistent, all RFIs and submittals are clear and complete, and the project is constructed to the contract requirements. The project consists of construction of approximately 1,500-ft of four-lane divided roadway, a 100-ft long bridge for KCS Railroad, approximately 2,700-ft of temporary shoofly track to enable continuous freight operations during construction, a new roadway underpass, retaining walls, and installation of permanent track and associated structures and sidewalks. Utility work includes storm drains, a pump station, force mains, gravity drains, and water line relocations.

#### **TxDOT | I-820 East Loop Project, Project Engineer\***

As part of the "Texas Clear Lanes Initiative," a plan put forth by the Governor to ease traffic along well-known chokepoints, this multi-phase project will reconstruct Interstate 820 along a three-mile stretch within the city limits of Fort Worth, Hurst, and Richland Hills. Dale handled weekly quantity reconciliation meetings with the owner to ensure estimate quantities and payments were accurately reported, facilitated weekly and monthly job financial cost report meetings, and coordinated four-week look-ahead and long-term scheduling.

**He worked with Servando on this project.**

#### **FDOT I-4 Ultimate, MOT Field Engineer\***

This project reconstructed and expanded 21 miles of tollway, including 140 bridges (13 widened, 74 replaced, and 53 newly constructed). Dale served as MOT field engineer for segment 1; he was responsible for coordination of lane closures and MOT project meetings and managed MOT crews during temporary traffic shifts. **He worked with Tammam on this project.**

\*Denotes project prior to joining Sundt.

### *Education / Certifications*

BS, Construction  
Engineering, Florida A&M  
University

OSHA 10 and 30-Hour  
First Aid and CPR Training

### *7 Years of Experience*

TXDOT US 175 Widening, Henderson/Tyler



*Tab*

**E**

# PROJECT SCHEDULE AND SEQUENCE



# PROJECT SCHEDULE AND SEQUENCE

## PROJECT DELIVERY AND IMPLEMENTATION SCHEDULE

### Construction Sequencing

Sundt has developed a detailed project schedule based on the proposal due date of November 15th, 2023 and an assumed NTP on February 13, 2024, which is 90 days following the Proposal due date. Sundt, however, has local resources in place and is available to begin work sooner, if approved. Using our schedule, **Sundt will finish the project 80 days early, with final acceptance in 835 calendar days.**

The schedule’s work breakdown structure (WBS) includes key project milestones, submittals, permits/easements/ROW, mobilization, and construction. Construction activities are broken down by Phase 1 and Phase 2, based on the limits shown in the sequencing plan. Each construction phase includes activities for preparing the ROW/clearing/SWPPP, drainage and utility, roadway, and bridge structures detailed by type of work. Illumination, irrigation and conduit activities are also included, as necessary.

Once all work is completed, we will install pavement markers and signs and make the roadway ready to open to traffic.

Upon notice of award, Sundt and our subcontractors and suppliers will begin preparing material submittals and shop drawings to be submitted to the City prior to starting construction. We have also included fabrication time for long-lead materials in the schedule. At least 30 days prior to mobilization, Sundt will notify all affected property owners via certified mail and obtain written letters of approval prior to final acceptance. Our approach to the project meets all USACE Individual Permit #SWF-2018-00485 Requirements and we will review all details with the City prior to starting construction.

Sundt’s schedule includes a 90-day activity following NTP for the City of Lewisville to obtain all necessary easements for construction activities. Our early completion includes this three-month delay so that a delayed start is not necessary and there is no cost impact to the City. However, **if the City obtains access sooner than 90 days, an additional 50 days of schedule savings is possible.**

GENERAL ROADWAY SEQUENCE OF EVENTS	GENERAL STRUCTURES SEQUENCE OF EVENTS
Install MOT and advanced warning signs	Build bridge access
Clear, grub, and prep ROW for access to site	Install drilled shafts
Install SWPPP and erosion control BMPs	Construct MSE Retaining Wall
Sawcut and remove concrete as shown on plans, including ‘sidewalk closed’ signage and barricades to protect pedestrians	Form, pour, strip abutments, columns, caps
Excavate, lay, backfill storm drainage lines and structures	Install concrete girders
Excavate, lay, backfill water lines. Complete bores as shown.	Install PMD, precast concrete panels, and overhangs
Excavate, lay, backfill sanitary sewer relocation	Install bridge deck SEJ’s, rebar, and place decks
Excavate and embank to top of subgrade, including excavation of borrow material from Mitigation Area	Form, pour, and strip bridge approach slabs – follows roadway subgrade

## SECTION E / PROJECT SCHEDULE AND SEQUENCE

GENERAL ROADWAY SEQUENCE OF EVENTS	GENERAL STRUCTURES SEQUENCE OF EVENTS
Install conduit below pavement	Install bridge-mounted illumination conduit and steel water line
Lime treat six-inch subgrade	Form, pour, and strip C402 rail
Place ten-inch concrete pavement	Install metal C402 railing and pedestrian rail
Embank for sidewalks and median	Install bridge-mounted luminaries
Install conduit within median and along sidewalks	Place bridge raised median
Place sidewalk, curb ramps, and median pavement	
Install streetlight foundations, luminaries	
Install illumination and irrigation services and controllers	
Finish slopes and place topsoil with hydromulch/blanket	

Following NTP, our plan is to begin work west of the Elm Fork Trinity River, progressing clearing, drainage, and water line installation from Railroad Street to the Elm Fork Trinity River. Once the City obtains the necessary easements to access the east side of the river, work including clearing and bridge access will be completed. Sundt’s crews will be trained on the USACE Individual Permit requirements and will place temporary orange construction fence delineating the wetlands that cannot be disturbed outside of the permitted area. Our crews will maintain the existing two-way traffic at Huffines Boulevard, as our primary access to the east side of the Trinity River is shared with the main entrance/exit to Camelot Landfill.

The channel bridge will be constructed with a single structures crew allowing the drilled shafts to be advanced on the Trinity River bridge. Once the drilling is sufficiently progressed, columns will begin at Bent 2 working towards Bent 19. A second substructure crew will follow the columns completing the bent caps. Bridge beam erection will begin at Span 2 crossing the Elm Fork Trinity River following the requirements outlined on the River Access Sequencing plan to protect people using the river. This includes placement of signage at the SH 121 and LLELA boat ramps and closures only for setting concrete girders, deck form placement and removal, and the deck concrete pour. Our schedule includes a Boating Season activity restricting

closures from April 1st to October 31st of each year. Superstructure will continue with crews working from Unit 2 to Unit 10. Span 1 will be constructed once the embankment is completed at Abutment 1.

Concrete paving will begin once the bridge deck is completed to allow for an efficient paving operation, with sidewalks, median and roadway finishes to follow. Once the concrete paving is completed within the Phase 1 work area, MOT devices and signs will be installed per the Traffic Control Plan for Phase 2. Two-way traffic will be maintained at Railroad Street, with this work being phased to be completed at the end of the project.



Figure 1: Intersection of Corporate Drive and Railroad Street (Phase 2)

A summary-level schedule is shown on the following page as Figure 2, and a complete initial CPM schedule is provided at the end of the section.

# SECTION E / PROJECT SCHEDULE AND SEQUENCE

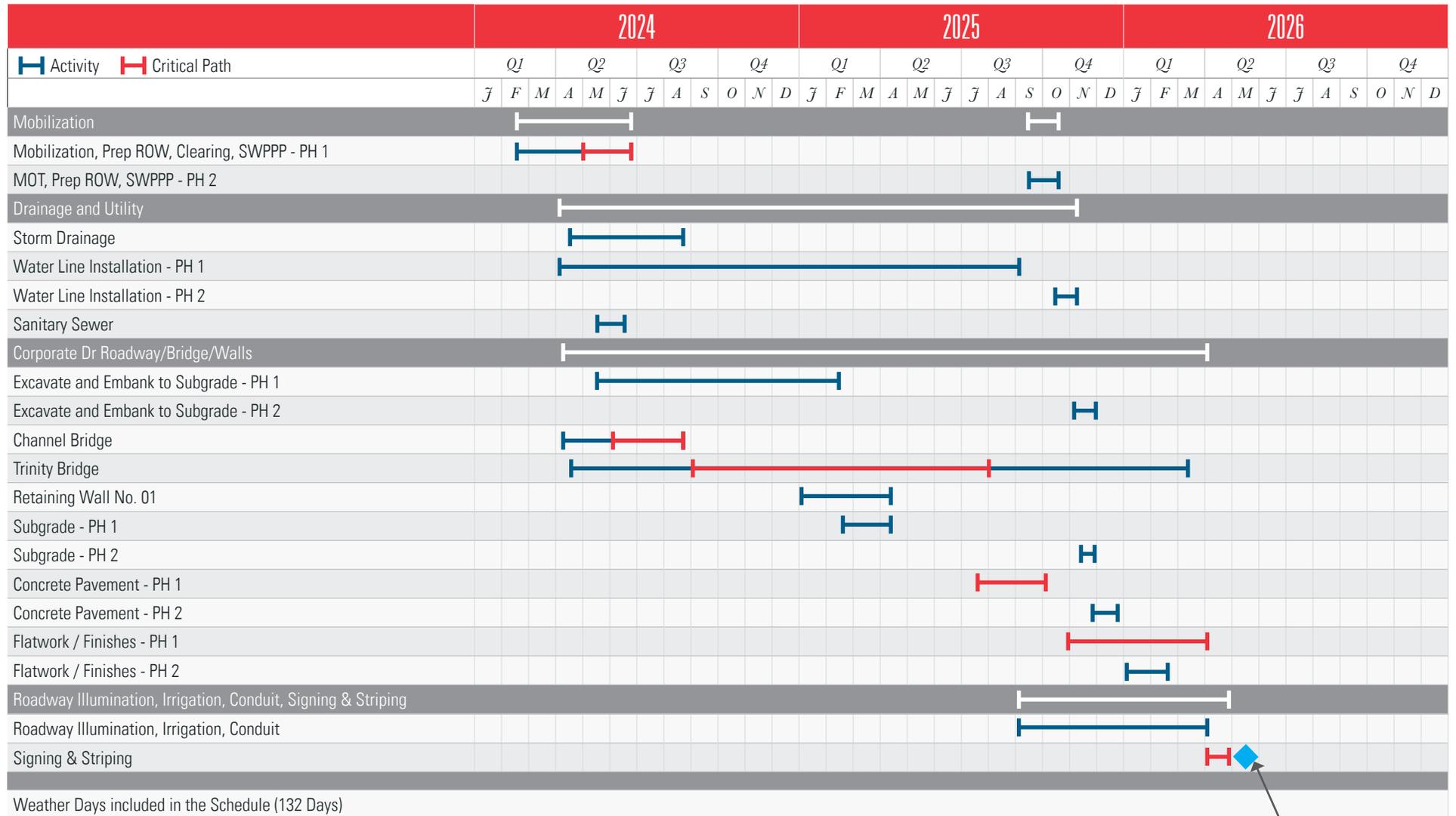


Figure 2: Summary schedule

Final Acceptance  
(835 Calendar Days)

## SECTION E / PROJECT SCHEDULE AND SEQUENCE

### Critical Path

Sundt's Critical Path Method (CPM) schedule shows critical activities on the project that must be completed on time for the project to stay on schedule. We have identified that by completing the Channel Bridge first, Sundt can focus our resources on the Trinity River Bridge once we have access east of the river. The project's critical path initially runs through the access and clearing from the Trinity River to Huffines Boulevard, followed by the Channel Bridge construction, and then the construction of the Trinity Bridge. Once the bridge deck is completed, the critical path shifts to the roadway's concrete pavement.

The critical path continues with the installation of the sidewalk from Railroad Street to the end of the alignment. We will then place pavement markers and install signs to finish the new roadway. Once the road is ready to open to traffic, substantial completion is reached, with only punch list activities remaining to gain final acceptance.

Critical path activities are shown in red on the summary-level schedule on the prior page and the complete CPM schedule at the end of the section.



Figure 3: Trinity River Bridge Crossing

## PROJECT-SPECIFIC TASKS

### Drainage Improvements

Drainage improvements consist of constructing a new closed drainage system and extending the existing 4x3-ft RCB culvert (Culvert "EX1") crossing between Waste Management ROW. Line A will be completed first, followed by Line B and Line C. The storm drain lines are to be installed prior to the water line and embankment, except at Valley Ridge Boulevard where Water Line B is to be installed prior to the Storm Drain Line C trunkline. Each drainage line outfalls into existing detention ponds and completing the end treatments efficiently will be important to maintain positive drainage to the site.

In addition to the storm drain lines, Sundt will be expanding the southwest quadrant of the existing Valley Storage area to Elevation 444.0 to mitigate the loss of storage due to the roadway construction. Although the bottom of the Valley Storage Mitigation Pond is just above the normal water elevation, monitoring the weather conditions and proper placement of BMP's will be important to avoid impacts due to rainfall. The excavation of the mitigation area will be sequenced to occur with the roadway embankment activities to use the excavated material as borrow fill.

### Water and Sanitary Sewer Improvements

The project includes a proposed 12-in water line (Water Line A) which runs the length of the alignment, with a proposed crossing at Valley Ridge Boulevard (Water Line B) and future stub-out east of the Trinity Bridge (Water Line C). The water line will be primarily installed via open cut with a minimum cover of 48-in. A bore will be completed crossing the existing channel parallel to the channel bridge, with other bores at Huffines Boulevard and Railroad Street. Along the Trinity Bridge, the 12-in water line transitions from PVC to Schedule 40 Epoxy Lined/Coated welded steel pipe. The welded steel pipe is supported from hangers under the Trinity bridge using single-roller and double-roller pipe supports.

The relocation of Water Line H near Abutment 1 of the Trinity Bridge is a key activity necessary to relocate the existing gravel maintenance road and complete the embankment at the bridge

## SECTION E / PROJECT SCHEDULE AND SEQUENCE

abutment. ***Sundt's project team, including Project Manager Tammam Nashad, General Superintendent Noel Estrado, and Project Engineer Dale Kerr, is experienced in working with the City's engineering and utility inspectors and will coordinate all required testing, shutdowns, and connections.***

Work requiring inspection will be scheduled to avoid Friday afternoons and the weekends to accommodate City personnel.

The existing 24-in sanitary sewer located at the Trinity Bridge Abutment 20 and MSE Wall No. 1 requires realignment and installation of new manholes prior to the installation of the abutment's drilled shafts and start of the retaining wall and embankment. Sundt will use bypass sewage pumping to avoid disrupting sewer flow during the construction of the new sewer main and grout fill the existing line following activation of the new line.

### **Grading and Paving**

Although a portion of roadway embankment has been previously placed by others, excavation and embankment will be necessary to the proposed top of subgrade following the drainage and utility installations. The subgrade will be prepared for six-in of lime treatment, which will be uniformly applied and mixed using a soil stabilizer/reclaimer.

Small areas such as the Railroad Street intersection may require lime trimmings or flex base instead of the six-in lime-treated subgrade. Sundt believes using flex base in lieu of the six-in lime-treated subgrade could provide additional benefit and can further develop this idea as a value engineering change upon award.

For concrete pavement construction, we will use a slip-form paver for machine-poured concrete, with steel paving forms for hand-pours. The pavement surface will be cured and protected before opening to traffic for at least 14-days or until concrete strength reaches 75% of the 3,500-psi design strength.

### **Bridge Construction**

The safe and efficient construction of the two bridges on the alignment will be essential to the project's success. Sundt has demonstrated on its



*Figure 4: Previously placed embankment from Railroad Street to Channel Bridge*

Windhaven Parkway project in Lewisville and other local projects that its management and crews understand the requirements of work from heights and in environmentally sensitive areas. Sundt has developed a bridge access plan that incorporates the requirements of the USACE Section 404 Permit and will manage the work at the river according to the River Access Sequencing Plan.

Sundt's drilled shaft subcontractor will complete test piers at each bridge to demonstrate adequate placement techniques, prior to starting the 24-in and 36-in diameter drilled shafts at the Channel Bridge. Drilled shafts will then continue with the 42-in diameter drilled shafts at Bent 2 of the Trinity Bridge, and then equipment will mobilize to the east side of the Trinity River. Sundt's substructure crews, led by Superintendent Noel Estrado, will then complete the 36-in diameter columns at Bent 2, with 42-in column sections for column heights greater than 18-ft. Substructure will continue east of the river concurrently with drilled shafts progressing towards Abutment 20.

Sundt's preconstruction team worked with Tammam and Noel to develop detailed crane lifting plans for each span of the two bridges. At the Channel Bridge, additional attention is required at the overhead powerlines, as the concrete beams will be set under the high voltage lines. The Trinity River beam set is shown in Figure 5, below. Sundt's superstructure

# SECTION E / PROJECT SCHEDULE AND SEQUENCE

crews plan to install Span 2 over the Trinity River utilizing a 300-ton crawler crane located on the east side of the river. The existing ground will be cut to create level access and restored to original contours following the construction of the bridge. **By not placing any material in the river, we will not impact boat traffic and the river will only need to be closed for the overhead erection of each beam.**

**Crane Lifting Plan - Span 2 - Trinity River Beam Set**

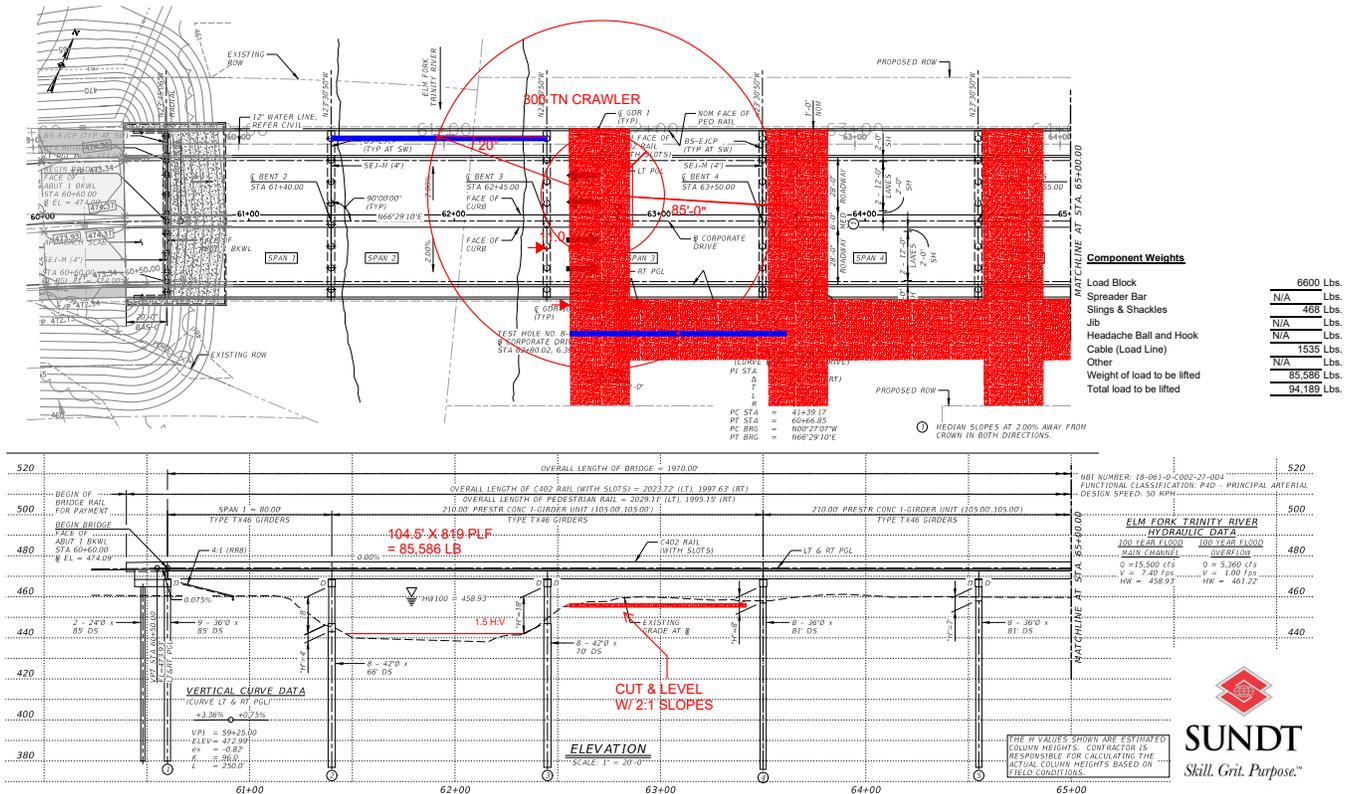


Figure 5: Trinity River beam set

## Retaining Wall

The construction of Retaining Wall No. 01 is a unique element of the project. Installation of the wall will follow the Trinity Bridge Abutment 20. Project Manager Tammam Nashad and Project Engineer Dale Kerr will work closely with Sundt’s MSE wall supplier to submit casting drawings, construction drawings, and design calculations to the City well in advance of the retaining wall construction, due to the long lead of the MSE wall design and casting.

Prior to beginning MSE wall installation, we will excavate the foundation for the wall and place flexible base for ground improvement. As each row of MSE panels is set, filter fabric will be installed behind the wall joints and reinforcing strips placed and backfilled. We will take care to prevent surface water from damaging the wall during construction

by preventing water from ponding on the backfill and against the face of the wall. Once the wall height reaches more than 6-ft, fall protection will be installed and moved as the wall advances vertically.

Once the top wall panels are installed and backfilled to subgrade, the coping level-up will be placed, coping installed, and coping anchorage placed. Sundt’s retaining wall crew will ensure proper fall protection is in place until the pedestrian rail and C-402 traffic rail is completed.

## Roadway Illumination, Irrigation Conduit, Signing, and Striping

Installation of underground conduits crossing the proposed pavement will occur prior to mixing lime for the treated subgrade. The remaining elements will be installed once all drainage and roadway elements are completed. Completion of street

## SECTION E / PROJECT SCHEDULE AND SEQUENCE

lighting, irrigation conduits, permanent signing, and striping will occur during the final stages of the project prior to Corporate Drive being ready to be opened to traffic. Sundt will coordinate with the electrical, signing, and striping subcontractors to perform the work safely and per the plans and specifications.



Figure 6: Beginning of Corporate Drive Segment 2

### MATERIALS OR WORK ITEMS THAT CAN IMPACT PROJECT COMPLETION DATE

Sundt's CPM schedule includes activities necessary to mobilize key equipment and materials, including:

#### Equipment

- Bridge structures equipment
- Excavation and grading equipment
- Concrete paving equipment

#### Materials

- Water pipe and valves
- Sanitary sewer pipe and manholes
- Drainage pipe and structures
- Bridge beams and precast deck panels
- MSE wall panels
- Traffic and pedestrian metal rail
- Illumination poles

Market instability has introduced uncertainty regarding lead times for both equipment and materials. Sundt addresses this challenge by maintaining a large self-perform equipment fleet, which enables us to bypass potential delays in equipment mobilization by utilizing in-house resources. Additionally, early procurement of long-lead materials and meticulous tracking of expected fabrication in the CPM schedule enable us to mitigate the impacts arising from the current market conditions.

## CASE STUDY



### *City of Lewisville Windhaven Parkway*

Sundt was able to overcome price escalations on the Windhaven Parkway Project through value engineering and partnering resulting in no additional costs to the City.

Other items out of our control, such as permitting, easements, and ROW acquisition are identified in the schedule, allowing for early detection of potential issues or delays. Sundt will work with the City to mitigate these issues to avoid impacts to the project completion date. As previously stated, Sundt's schedule includes an activity for the additional 90 days potentially needed by the City to secure easements east of the Trinity River, eliminating the need for a delayed start and avoiding any cost impacts to the project.

### CALENDAR, HOLIDAYS, AND WEATHER DAYS

Sundt's CPM schedule uses a five-day work week and includes the nine yearly holidays recognized by the City of Lewisville. Weather days have been calculated based on the NOAA 10-year average, with additional winter weather days allotted due to the critical work, primarily including bridge and concrete paving activities. This results in a total of 59 weather days per year, or 132 weather days for the project duration. Sundt's scheduling practice is to include the weather days within individual activities, using a project calendar, rather than a single activity at the end of the schedule to provide a more accurate representation of the project timeline, better manage weather-related risks, and plan the allocation of resources more effectively.

Activity Name	OD	Start	Finish	2024							2025							2026							2027										
				Oct	N	D	J	F	M	Apr	M	J	Jul	A	S	Oct	N	D	J	F	M	A	M	J	Jul	A	S	Oct	N	D	J	F	M	A	M
<b>City of Lewisville Corporate Dr (Segments 2 &amp; 3) - Proposal Schedule</b>																																			
<b>MILESTONES</b>																																			
Submit RFCSP	0	15-Nov-23		◆ Submit RFCSP																															
Notice of Award by City Council (City of Lewisville)	0	18-Dec-23		◆ Notice of Award by City Council (City of Lewisville)																															
NTP - Corporate Drive (Segments 2 & 3)	0	13-Feb-24		◆ NTP - Corporate Drive (Segments 2 & 3)																															
Phase 2 - Completion	0		25-Feb-26	◆ Phase 2 - Completion																															
Phase 1 - Completion	0		09-Apr-26	◆ Phase 1 - Completion																															
Project Substantial Completion - SUNDT	0		23-Apr-26	◆ Project Substantial Completion - SUNDT																															
Project Duration - Sundt (27.5 Months)	835	13-Feb-24	27-May-26	Project Duration - Sundt (27.5 Months)																															
Project Final Acceptance - SUNDT (835 CAL Days)	0		27-May-26	◆ Project Final Acceptance - SUNDT (835 CAL Days)																															
Project Final Acceptance - Contract (915 CAL Days)	0		15-Aug-26	◆ Project Final Acceptance - Contract (915 CAL Days)																															
<b>SUBMITTALS</b>																																			
Waterline Materials Submittal	16	18-Dec-23	11-Jan-24	☐ Waterline Materials Submittal																															
Sanitary Sewer Materials Submittal	16	18-Dec-23	11-Jan-24	☐ Sanitary Sewer Materials Submittal																															
Reinf. Concrete Pipe/Box Shop Drawing Submittal	16	20-Dec-23	16-Jan-24	☐ Reinf. Concrete Pipe/Box Shop Drawing Submittal																															
Precast Concrete Tx46/Tx54 Girders Shop Drawings Submittal	24	18-Dec-23	24-Jan-24	☐ Precast Concrete Tx46/Tx54 Girders Shop Drawings Submittal																															
Lighting Poles Shop Drawings Submittal	16	02-Jan-24	24-Jan-24	☐ Lighting Poles Shop Drawings Submittal																															
Traffic Rail Shop Drawings Submittal	24	18-Dec-23	24-Jan-24	☐ Traffic Rail Shop Drawings Submittal																															
Pedestrian Rail Shop Drawings Submittal	24	18-Dec-23	24-Jan-24	☐ Pedestrian Rail Shop Drawings Submittal																															
Drainage Inlet/Manhole Structures Shop Drawings Submittal	24	27-Dec-23	31-Jan-24	☐ Drainage Inlet/Manhole Structures Shop Drawings Submittal																															
Precast Concrete Deck Panels Shop Drawings Submittal	24	27-Dec-23	31-Jan-24	☐ Precast Concrete Deck Panels Shop Drawings Submittal																															
MSE Retaining Wall No. 1 Shop Drawings Submittal	32	18-Dec-23	05-Feb-24	☐ MSE Retaining Wall No. 1 Shop Drawings Submittal																															
<b>CITY OF LEWISVILLE</b>																																			
Review and Approval Waterline Materials Submittal	16	12-Jan-24	05-Feb-24	☐ Review and Approval Waterline Materials Submittal																															
Review and Approval Sanitary Sewer Materials Submittal	16	12-Jan-24	05-Feb-24	☐ Review and Approval Sanitary Sewer Materials Submittal																															
Review and Approval RCP/RCB Drawings	16	17-Jan-24	07-Feb-24	☐ Review and Approval RCP/RCB Drawings																															
Review and Approval Precast Conc. Girders Shop Drawings	16	25-Jan-24	15-Feb-24	☐ Review and Approval Precast Conc. Girders Shop Drawings																															
Review and Approval Lighting Poles Shop Drawings	16	25-Jan-24	15-Feb-24	☐ Review and Approval Lighting Poles Shop Drawings																															
Review and Approval Traffic Rail Shop Drawings	16	25-Jan-24	15-Feb-24	☐ Review and Approval Traffic Rail Shop Drawings																															
Review and Approval Pedestrian Rail Shop Drawings	16	25-Jan-24	15-Feb-24	☐ Review and Approval Pedestrian Rail Shop Drawings																															
Review and Approval Inlet Structures Shop Drawings	16	01-Feb-24	23-Feb-24	☐ Review and Approval Inlet Structures Shop Drawings																															
Review and Approval Precast Deck Panels Shop Drawings	16	01-Feb-24	23-Feb-24	☐ Review and Approval Precast Deck Panels Shop Drawings																															
Review and Approval MSE Wall Shop Drawings	16	06-Feb-24	28-Feb-24	☐ Review and Approval MSE Wall Shop Drawings																															
<b>PROCUREMENT / LONG LEAD ITEMS</b>																																			
Fabrication / Delivery Waterline Materials	15	06-Feb-24	20-Feb-24	☐ Fabrication / Delivery Waterline Materials																															
Fabrication / Delivery Sanitary Sewer Materials	15	06-Feb-24	20-Feb-24	☐ Fabrication / Delivery Sanitary Sewer Materials																															
Fabrication / Delivery RCP/RCB	15	08-Feb-24	22-Feb-24	☐ Fabrication / Delivery RCP/RCB																															
Fabrication / Delivery Tx/Tx 54 Precast Concrete Girders	30	16-Feb-24	16-Mar-24	☐ Fabrication / Delivery Tx/Tx 54 Precast Concrete Girders																															
Fabrication / Delivery Traffic Rail	30	16-Feb-24	16-Mar-24	☐ Fabrication / Delivery Traffic Rail																															
Fabrication / Delivery Pedestrian Rail	30	16-Feb-24	16-Mar-24	☐ Fabrication / Delivery Pedestrian Rail																															
Fabrication / Delivery Inlet/Manhole Structures	30	24-Feb-24	24-Mar-24	☐ Fabrication / Delivery Inlet/Manhole Structures																															
Fabrication / Delivery Precast Concrete Deck Panels	45	24-Feb-24	08-Apr-24	☐ Fabrication / Delivery Precast Concrete Deck Panels																															
Fabrication / Delivery Lighting Poles	60	16-Feb-24	15-Apr-24	☐ Fabrication / Delivery Lighting Poles																															



**Sundt Construction, Inc.**  
8445 Freeport Parkway  
Irving, TX 75063

**City of Lewisville Corporate Dr (Segments 2 & 3) - Proposal Schedule**  
Data Date 15-Nov-23 | Run Date 13-Nov-23

Actual Work
  Critical Remaining Work
  Remaining Work
  Milestone
  Remaining Level of Effort

















City of Lewisville Windhaven Parkway



*Tab*

**F**

**SAFETY PRACTICES AND  
RECORD FOR LAST 5 YEARS**



# SAFETY PRACTICES AND RECORD FOR LAST 5 YEARS

---

Sundt is committed to ensuring the Corporate Drive Segment 2 & 3 project is a safe environment for everyone. Sundt's formal construction safety program, 'Safety By Choice', gives equal emphasis to the "why" of safety, not just the "how." In addition to providing thorough, up-to-date construction safety training to our craft employees, we focus on the reasons to stay safe: their families, friends, health, and livelihood. Sundt will provide a safe working environment and ensure all staff are properly trained and equipped to perform their jobs without injury or increasing risk to themselves or others.

Every project is different. Different site logistics, environmental conditions, and then different work elements, equipment, and potential risks and exposures. Because of this, every project team develops a project-specific safety plan which provides guidance, procedures, and work activities required in the planning, communication, and execution of work. It is a dynamic document that is made available to all persons involved, who must understand and comply with the project requirements. When developing the project-specific safety plan, we review all requirements (Sundt's and the City's) to verify our plan meets or exceeds all safety requirements. All persons assigned to perform any portion of the work must attend a project-specific safety orientation to acquaint them with potential hazards, safety policies and practices, and general safety rules.

### Sundt's Safety Management Philosophy

Sundt's safety philosophy is industry leading in many ways. We have developed the S#! That Can Kill You (STCKY) concept after evaluating how our industry measures safety. Since OSHA was created in the 1970s, the construction industry has been in pursuit of "zero injuries." Disappointingly, construction continues to experience the most fatalities of any industry, with fatalities remaining

## SUNDT SAFETY BY THE NUMBERS

---

# .60

### EMR Average

*Well below the industry average of 1.00*

---

### Sh\*t That Can Kill You (STCKY™)

*Sundt's new safety framework, focused on educating our workforce and trade partners to be more aware of fatal hazards.*

---

# 91% DROP

### In Recordable Incident Rate (RIR) Since 2000

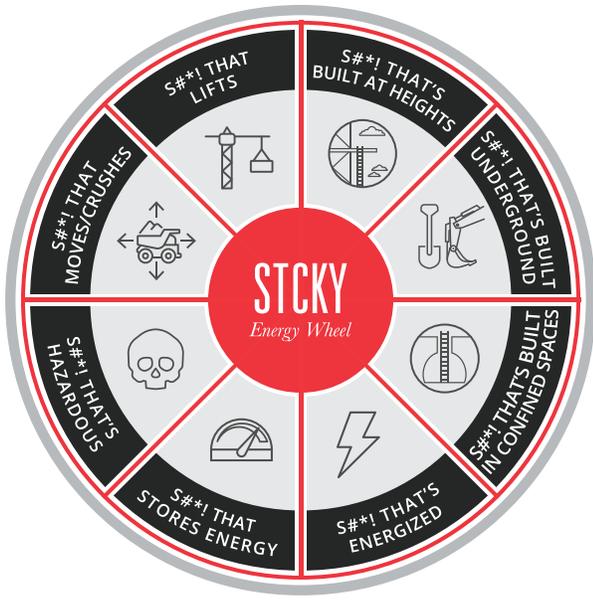
---

# 1 OF 2

### Companies to win multiple AGC National Construction Safety Excellence Awards

---

consistent over the last 10 years. This raises some important questions about whether we are focusing on the correct things to drive down the frequency of severe incidents, not just the overall incident rate; and we have concluded that, as an industry, we are not. As we move forward, we will continue to have a daily goal of zero injuries, but also a heightened focus on enhanced training, awareness, and mitigation of the most common fatal hazards on a jobsite, known as the 'Fatal Eight.'



**Safety Management**

During the construction phase, Sundt will provide qualified and experienced safety professionals to manage the safety program and achieve the objectives and target defined by the project. Contractors and subcontractors will provide sufficient safety professionals within their own organizations to cover all areas and activities, according to their staffing levels and project requirements. Additionally, each superintendent is responsible for implementing and enforcing compliance to the project safety program, practices, and procedures in their areas of responsibility.

**Sundt Safety Approach**

Sundt has a strong safety culture and a history of making safety the number one project priority for our team. We have a proven track record implementing successful safety programs. Creating a leading safety culture for the Corporate Drive Segment 2 & 3 project will require a consistent and energetic approach, which comes from top management and supervision and empowers all employees to take ownership of our safety approach.

**Plan the Work, Work the Plan**

Our projects conduct preparatory meetings, pull planning, three-to-six-week look-aheads, and

trade coordination meetings, in addition to scope-specific work packages and daily task hazard analysis (THA) planning meetings. These processes have been proven effective at identifying and mitigating risk—especially risks from trade stacking and overhead work. The people closest to the work are involved in these processes and have decision-making power on how work is performed, which positively affects safety. Our project manager and superintendent ensure adequate participation from each stakeholder involved in these processes.

**Coaching Our Teams**

We have a robust safety training system for our operations staff that ensures completion of mandatory training. Our corporate safety trainer monitors this system and oversees its evolution.

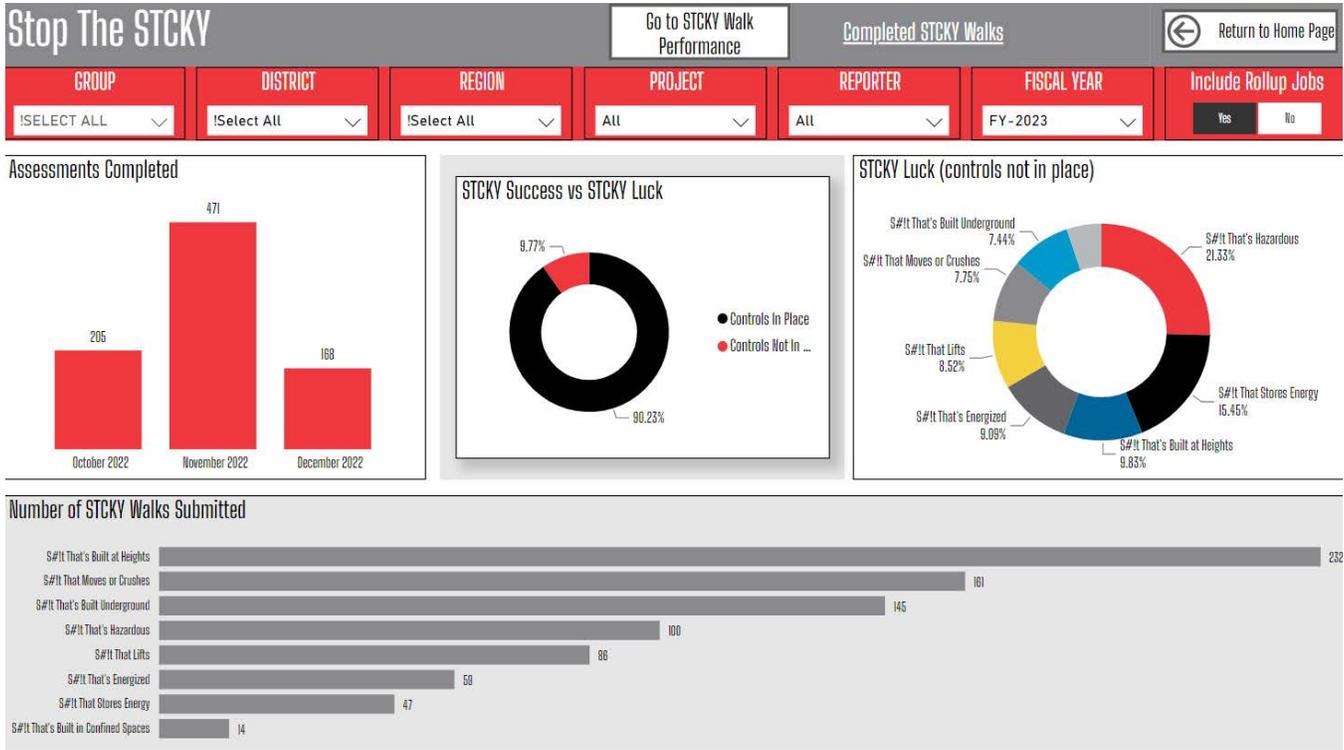
We developed a training video database specific to the processes and equipment used by our craft. When new crew members come to one of our sites, they attend an orientation which consists of impactful videos and messaging, leaving them empowered to work safely, speak up, and listen up. We consistently validate that our training efforts are effective via field inspections and weekly safety quizzes. Each week, a safety question is emailed to all operations employees. We use dashboard reporting tools, as shown on the next page, to track the results and indicates where training needs enhancement.

**SAFETY EXECUTION**

**Safety Inductions/Guidelines**

All new project employees will receive a safety induction course. This course will last one half day and include general rules such as driving, safety procedures, drugs and alcohol prohibition, environmental awareness, emergency plans, and more. Upon completion of this course, a pocket-sized safety project handbook with specific excerpts from the safety manual will be distributed to all personnel. This handbook will include a signature page for the employee's acknowledgment of receipt.

# SECTION F / SAFETY PRACTICES AND RECORD FOR LAST 5 YEARS



## Owner, Visitor & Community Safety

Our focus on safety includes everyone who may be influenced by our work, including nearby residents, businesses, visitors and travelers. Performing major construction in urban environments requires constant vigilance to ensure safety remains the priority. Measures we take include holding daily morning safety briefings; providing information to the community about potential dangers; ensuring that work sites are fenced and secure; using spotters for reversing vehicles; clearly signing changes to traffic patterns; working with firefighters and emergency services to ensure continuous access; appropriate use of lights, security cameras and security guards; and keeping streets around worksites neat and clean.

We will communicate with Waste Management and Camelot Landfills, and other property owner's adjacent to the project site to coordinate ingress/ egress points and traffic control measures. Informing the public about risk associated with increased truck traffic and construction activity near their businesses is an important part of project safety. We routinely minimize noise,

vibration, dust, settlement and other potential consequences of construction.

Everyone entering the site will be required to check-in and out, pass any required training and receive proper personal protective equipment before entering the site. All visitors must be accompanied by workers familiar with the site.

## Safety Training

Continuing education will be provided for supervisors and engineers. At project start-up, we conduct a 30-hour safety training program that includes all of the management and supervision team. Monthly supervisor safety meetings are held during the project to provide additional training. Attendance and participation is a mandatory requirement for supervision. All of this is in addition to the mandatory, project-specific safety orientation.

## Daily Safety Instruction

A daily safety meeting is conducted at the beginning of each shift. THAs are the backbone of our construction planning approach. THAs are specific to various work disciplines and detail

## SECTION F / SAFETY PRACTICES AND RECORD FOR LAST 5 YEARS

field operations according to task, hazards, and mitigation strategies that will be implemented. After reviewing the THA, foremen prepare daily work plans for safe execution of work that considers location and operation-specific hazards.

For example, separate THAs for items such as concrete pump safety, working near water, and crane and rigging. The daily safety meeting also covers haul and material delivery coordination. A comprehensive “toolbox” safety meeting is also conducted at the start of each work week.

### Relentless Housekeeping

Each project team writes a cleanliness plan and is expected to maintain “relentless housekeeping,” which massively drives worker engagement by showing workers we care for them and want to provide them with a clean workplace. It also drives worker participation and involvement are because they are provided with the resources and empowerment to take ownership of the site’s condition.

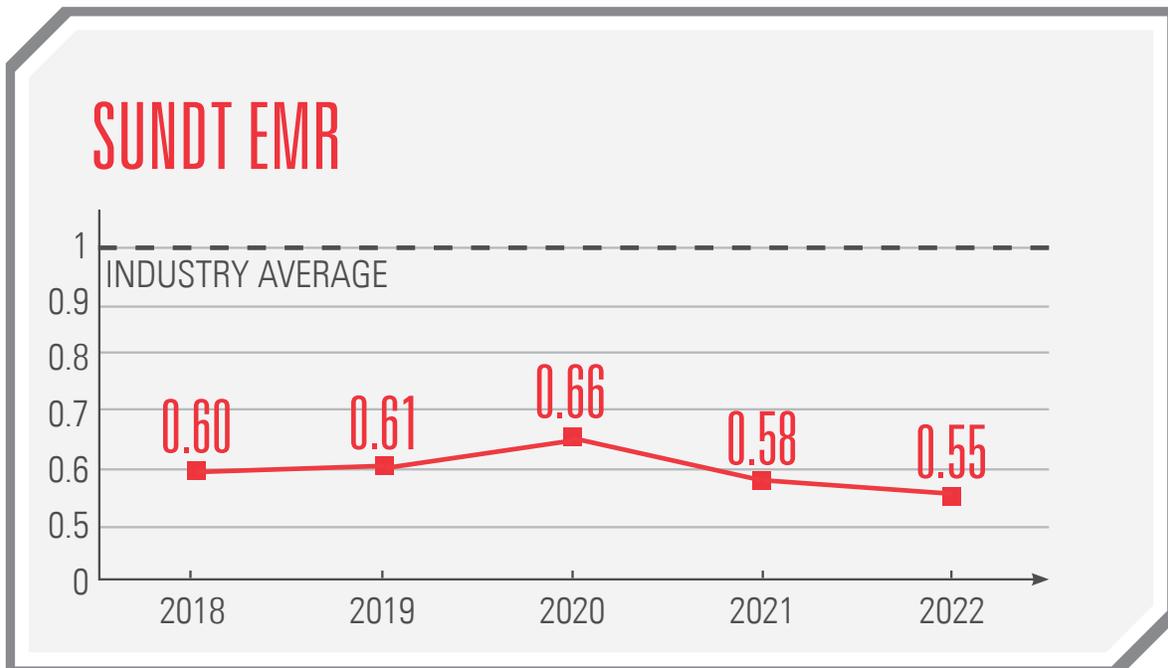
### Jobsite Walk-Throughs

Supervisors are required to complete jobsite safety walks weekly, depending on their role. Our personnel input safety observations, incidents, and audits into our safety app. The data is displayed on a digital dashboard which identifies risks that we can effectively target with corrective action plans.

## EXPERIENCE MODIFICATION RATE (EMR)

Sundt’s corporate safety program and project-level safety planning, training, and implementation are second to none. As evidenced by our ratings, we take pride in ensuring the public is protected from ongoing construction activities and our employees return home safely every day.

Sundt’s EMR for each of the last five years, provided by Zurich American Insurance Company, are provided below.



TxDOT SH 31 Corsicana Relief Route



*Tab*

**G**

# OWNER-CONTRACTOR AGREEMENT & GENERAL CONDITIONS



*Section G*

# OWNER-CONTRACTOR AGREEMENT & GENERAL CONDITIONS

---

Sundt has reviewed the City-Contractor Agreement and the Agreement is acceptable as is.



SUNDT

*Skill. Grit. Purpose.*<sup>®</sup>

***PROPOSER'S ACCEPTANCE OF EVALUATION METHODOLOGY***

**WAIVER OF CLAIMS: EACH PROPOSER BY SUBMISSION OF A RESPONSE TO THIS COMPETITIVE SEALED PROPOSAL ("CSP") WAIVES ANY CLAIMS IT HAS OR MAY HAVE AGAINST THE OWNER, ITS EMPLOYEES, OFFICERS, AGENTS, REPRESENTATIVES, AND THE MEMBERS OF OWNER'S GOVERNING BODY, CONNECTED WITH OR ARISING OUT OF THIS RFP, INCLUDING THE ADMINISTRATION OF THE RFP AND THE RFP EVALUATION. SUBMISSION OF A PROPOSAL INDICATES PROPOSER'S ACCEPTANCE OF THE EVALUATION TECHNIQUE AND PROPOSER'S RECOGNITION THAT SOME SUBJECTIVE JUDGMENTS MUST BE MADE BY THE OWNER DURING THE EVALUATION OF PROPOSALS. WITHOUT LIMITING THE GENERALITY OF THE FOREGOING, EACH PROPOSER ACKNOWLEDGES THAT THE BASIS OF SELECTION AND THE EVALUATIONS SHALL BE MADE PUBLIC AFTER THE CONTRACT IS AWARDED AND WAIVES ANY CLAIM IT HAS OR MAY HAVE AGAINST THE ABOVE-NAMED PERSONS, DUE TO INFORMATION CONTAINED IN SUCH EVALUATIONS.**

**Firm Name:** Sundt Construction, Inc.

**Date:** 11.15.2023

**Authorized Signature:**

  
\_\_\_\_\_

**Printed Name:**

Jeff J. Williamson, Group President

**CITY OF LEWISVILLE  
PURCHASING DIVISION**

**ADDITIONAL TERMS**

**ANTI-LOBBYING PROVISION**

During the period between proposal / sealed bid submission date and the contract award, proposers, including their agents and representatives, shall not directly discuss or promote their proposal with any member of the City of Lewisville City Council or City staff except during City-Sponsored inquiries, briefings, interviews, or presentations, unless requested by the City.

This provision is not meant to preclude offerors from discussing other matters with City Council members or City staff. This policy is intended to create a level playing field for all potential offerors, assure that contract decisions are made in public, and to protect the integrity of the RFP / Bid Evaluation process. Violation of this provision may result in rejection of the offeror's proposal.

**LAWS AND ORDINANCES**

Laws and Ordinances: The Contractor shall always observe and comply with all Federal, State and local laws, ordinances and regulations which in any manner affect the Contract or the work and shall indemnify and save harmless the City against any claim arising from the violation of any such laws, ordinances and regulations whether by the Contractor or his employees.

**PROTECTION OF RESIDENT WORKERS**

Protection of Resident Workers: The City of Lewisville actively supports the Immigration and Nationality Act (INA) which includes provisions addressing employment eligibility, employment verification, and nondiscrimination. Under the INA, employers may hire only persons who may legally work in the United States (i.e., citizens and nationals of the U.S.) and aliens authorized to work in the U.S. The employer must verify the identity and employment eligibility of anyone to be hired, which includes completing the Employment Eligibility Verification Form (I-9). The Contractor and its Subcontractors shall establish appropriate procedures and controls so no services or products under the Contract Documents will be performed or manufactured by any worker who is not legally eligible to perform such services or employment. The City reserves the right to audit Contractor's or Subcontractor's employment records to verify the existence of a completed Employment Eligibility Verification Form (I-9) for every worker performing services or manufacturing products under the Contract Documents. The audit will be at the City's expense.

**IMMIGRATION REFORM AND CONTROL ACT**

Immigration Reform and Control Act (8 U.S.C. §1324a): The City of Lewisville supports the Immigration Reform and Control Act (IRCA) which is a comprehensive scheme prohibiting the employment of unauthorized aliens in the United States. The Contractor shall submit a declaration signed under penalty of perjury of the laws of the State of Texas stating that it has not been found in violation of IRCA by the United States Attorney General or Secretary of Homeland Security in the preceding five (5) years. The Contractor shall ensure that its Subcontractors submit a declaration signed under penalty of perjury of the laws of the State of

Texas stating that they have not been found in violation of IRCA by the United States Attorney General or Secretary of Homeland Security in the preceding five (5) years. The Contractor and its Subcontractors shall at all times during the term of the contract with the City comply with the requirements of IRCA and shall notify the City within fifteen (15) working days of receiving notice of a violation of IRCA. The City may terminate a contract with the Contractor if the City determines that (a) the Contractor or its Subcontractors have been untruthful regarding IRCA violations in the preceding five (5) years; (b) if the Contractor fails to ensure that its Subcontractors submit the aforementioned declaration; or (c) the Contractor or its Subcontractors fail to timely notify the City of an IRCA violation.

**Sundt Construction, Inc.**

Contractor Name



Authorized Signature

**11.15.2023**

Date

**CITY OF LEWISVILLE  
PURCHASING DIVISION**

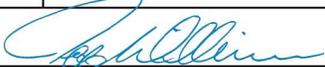
**STATE RECIPROCAL REQUIREMENT**

The City of Lewisville, as a governmental agency of the State of Texas, may not award a contract for general construction, improvements, services or public works projects or purchases of supplies, materials, or equipment to a non-resident bidder unless the non-resident's bid is lower than the lowest bid submitted by a responsible Texas resident bidder by the same amount that a Texas resident bidder would be required to underbid a non-resident bidder to obtain a comparable contract in the state in which the non-resident's principal place of business is located (Section 2252.002 of the Government Code). Bidder shall answer all the following questions by encircling the appropriate response or completing the blank provided.

1. Where is your principal place of business? Tempe, Arizona
  
2. Only if your principal place of business is not in the state of Texas, please indicate:
  - A. In which state is your principal place of business located? Arizona
  
  - B. Does that state favor resident bidders (bidders in your state) by some dollar increment or percentage?  YES  NO
  
  - C. If "YES", what is that dollar increment or percentage? \_\_\_\_\_

**NON-COLLUSION STATEMENT**

The undersigned affirms that they are duly authorized to execute this contract, that this company, corporation, firms, partnership or individual has not prepared this bid in collusion with any other Bidder, and that the contents of this bid as to prices, terms or conditions of said bid have not been communicated by the undersigned nor by any employer or agent to any other person engaged in this type of business prior to the official opening of this bid.

Vendor:	Sundt Construction, Inc.		
Address:	8445 Freeport Parkway Suite 240		
City, State, Zip:	Irving, Texas 75063		
Phone:	469.886.6600		
Email:	jjwilliams@sundt.com		
Bidder (Print Name):	Jeff J. Williamson		
Bidder Signature:			
Job Title:	Group President		
Signature of company official authorizing this bid:			
Company Official (Print name):	Jeff J. Williamson		
Job Title:	Group President		

**PROPOSED SUBCONTRACT BREAKDOWN**

1. Subcontractor ATS Drilling Inc  
Address 6559 Midway Road, Fort Worth, TX 76117  
Phone # 817-498-0040  
Description of Work Drill Shafts
  
2. Subcontractor TGW Land Services  
Address 10236 FM 274, Ravenna, TX 75476  
Phone # 978-604-4915  
Description of Work Clearing and Grub
  
3. Subcontractor Metroplex Pavement Markings  
Address 1001 Kennedy Ln Bldg #6, Saginaw, TX 76131  
Phone # 972-790-2983  
Description of Work Striping and Signing
  
4. Subcontractor Bean Electrical, LLC  
Address 821 E. Enon Avenue, Fort Worth, TX 76140  
Phone # 817-561-7400  
Description of Work Electrical work

Reference is made to Item 108.5 on Page SS-11

**PROPOSED SUBCONTRACT BREAKDOWN**

5. Subcontractor Texas Environmental Management, Sa, Inc  
Address PO BOX 369, Justin, TX 76247  
Phone # 940-648-3640  
Description of Work Erosion Control
6. Subcontractor Toro Bravo  
Address 7128 Envoy Ct, Dallas, TX 75247  
Phone # 469-369-2102  
Description of Work Rebar Tying
7. Subcontractor Scott Derr Painting Co  
Address 601 Bay Star Blvd., Webster, TX 77598  
Phone # 281-480-0158  
Description of Work Concrete Painting
8. Subcontractor \_\_\_\_\_  
Address \_\_\_\_\_  
Phone # \_\_\_\_\_  
Description of Work \_\_\_\_\_

Reference is made to Item 108.5 on Page SS-11

**LIST OF SUPPLIERS**

1. Item Supplied Aggregates  
Supplier Martin Marietta  
Address 1503 LBJ Freeway, Suite 400, Dallas TX, 75234  
Phone # 912-647-3978
  
2. Item Supplied Concrete Precast Items – RCP, Boxes etc.  
Supplier Rinker Materials  
Address 1000 N. Macarthur Blvd Bldg#7, Grand Prairie, TX 75050  
Phone # 214-864-5436
  
3. Item Supplied Ready Mix Concrete  
Supplier Smyrna Ready Mix Concrete  
Address 1225 S. Railroad St., Lewisville, TX 75057  
Phone # 817-329-8550
  
4. Item Supplied Lime Slurry  
Supplier U.S. Lime  
Address 1750 Brennan Ave, Fort Worth, TX 76106  
Phone # 817-223-3265

**LIST OF SUPPLIERS**

- |    |               |  |
|----|---------------|--|
| 5. | Item Supplied | <u>Misc Construction Materials, Rebar</u>                              |
|    | Supplier      | <u>CMC Construction Services</u>                                       |
|    | Address       | <u>3280 N. Freeway, Fort Worth, TX 76131</u>                           |
|    | Phone #       | <u>972-877-3193</u>  |
| 6. | Item Supplied | <u>Bridge Decking</u>  |
|    | Supplier      | <u>S.I.P Incorporated</u>  |
|    | Address       | <u>2204 Chestnut Street, Gadsden, AL 35904</u>                         |
|    | Phone #       | <u>256-546-5858</u>  |
| 7. | Item Supplied | <u>MSE Walls</u>   |
|    | Supplier      | <u>The Reinforced Earth Company (RECo)</u>                             |
|    | Address       | <u>9001 Airport Freeway, Suite 800, North Richland Hills, TX 76180</u> |
|    | Phone #       | <u>817-283-5503</u>  |
| 8. | Item Supplied | <u>C402 and Pedestrian Rail</u>  |
|    | Supplier      | <u>Larwel Industries</u>   |
|    | Address       | <u>PO Box 557, Bedford, TX 76095</u>                                   |
|    | Phone #       | <u>817-491-1200</u>  |

**LIST OF SUPPLIERS**

- |     |               |  |
|-----|---------------|--|
| 9.  | Item Supplied | <u>Waterline/Sewer Materials</u>                         |
|     | Supplier      | <u>ACT Pipe &amp; Supply Inc.</u>                        |
|     | Address       | <u>2585 NE 28<sup>th</sup> St., Fort Worth, TX 76111</u> |
|     | Phone #       | <u>817-831-1663</u>                                      |
| 10. | Item Supplied | <u>Precast Girders</u>                                   |
|     | Supplier      | <u>Texas Concrete Partners, L.P</u>                      |
|     | Address       | <u>PO BOX 338, Elm Mott, TX 76640</u>                    |
|     | Phone #       | <u>254-822-1351</u>                                      |
| 11. | Item Supplied | <u>Precast Concrete Panels</u>                           |
|     | Supplier      | <u>Austin PreStress</u>                                  |
|     | Address       | <u>7300 US 183 S., Austin, TX 78744</u>                  |
|     | Phone #       | <u>512-322-4000</u>                                      |
| 12. | Item Supplied | <u>_____</u>   |
|     | Supplier      | <u>_____</u>   |
|     | Address       | <u>_____</u>   |
|     | Phone #       | <u>_____</u>   |

# VENDOR SUPPLEMENTAL INFORMATION

*The following information is required for contract development.*

1. In what state was your business formed? Arizona
2. Provide the following information for the person authorized to execute contracts on behalf of your organization:

Name Jeff J. Williamson Title Group President  
Email Address jjwilliams@sundt.com Telephone No. 480.293.3237  
Mailing Address 2620 S. 55th Street City Tempe State AZ Zip 85282

3. Provide the following information for the contact person authorized to implement this contract on behalf of your organization:

Name Jeff J. Williamson Title Group President  
Email Address jjwilliams@sundt.com Telephone No. 480.293.3237  
Mailing Address 2620 S. 55th Street City Tempe State AZ Zip 85282

4. Provide the following information for the person authorized to receive notices and communications regarding this contract on behalf of your organization:

Name Kevin Graf Title Project Director  
Email Address kmgraf@sundt.com Telephone No. 512.402.4916  
\*Physical Business Address 8445 Freeport Parkway, Suite 240 City Irving State TX Zip 75063  
*\*Notices and communications will be mailed to this physical address*

5. Select and complete one of the following:

- a.  **Sole Proprietorship**
- i. Legal name of Sole Proprietor: \_\_\_\_\_
- ii. Physical business address: \_\_\_\_\_
- City \_\_\_\_\_ State \_\_\_\_\_ Zip \_\_\_\_\_
- b.  **General Partnership**
- i. Legal name of Partnership: \_\_\_\_\_
- ii. Physical business address: \_\_\_\_\_
- City \_\_\_\_\_ State \_\_\_\_\_ Zip \_\_\_\_\_

**[REMAINDER OF PAGE INTENTIONALLY LEFT BLANK]**

# VENDOR SUPPLEMENTAL INFORMATION

- c.  **Limited Partnership**
- i. Legal name of Limited Partnership: \_\_\_\_\_
- ii. General Partner(s):
- If a legal entity, name of the entity: \_\_\_\_\_
  - If an individual, name of the individual: \_\_\_\_\_
- iii. Physical business address: \_\_\_\_\_
- City \_\_\_\_\_ State \_\_\_\_\_ Zip \_\_\_\_\_

- d.  **Corporation**
- i. Legal name of Corporation: Sundt Construction, Inc.
- ii. Physical business address 2620 S. 55th Street
- City Tempe State AZ Zip 85282

- e.  **Limited Liability Company**
- i. Legal name of Limited Liability Company: \_\_\_\_\_
- ii. Physical business address \_\_\_\_\_
- City \_\_\_\_\_ State \_\_\_\_\_ Zip \_\_\_\_\_

- f.  **Other Entity (not listed)**
- i. Legal name and type of Company: \_\_\_\_\_
- ii. Physical business address \_\_\_\_\_
- City \_\_\_\_\_ State \_\_\_\_\_ Zip \_\_\_\_\_

6. Does your business have 10 or more full-time employees?  No  Yes

7. a. Are you a publicly traded business?  No  Yes – where traded: \_\_\_\_\_

b. Are you a wholly owned subsidiary of a publicly traded business?  No  Yes – which publicly traded business: \_\_\_\_\_

8. a. Is your business registered with the Texas Secretary of State?  No  Yes

b. If yes, please provide records or screenshot(s) from the Texas Secretary of State's website reflecting the name or names for which your business has been registered.

In signing this form, I acknowledge that I have read the above and state that the information contained therein is true and correct.

Signature:  Date: 11.15.2023

Print Name: Jeff J. Williamson Print Title: Group President



## Office of the Secretary of State

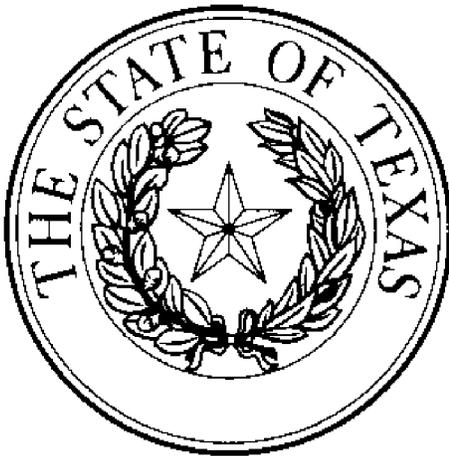
The undersigned, as Secretary of State of Texas, does hereby certify that the attached is a true and correct copy of each document on file in this office as described below:

SUNDT CONSTRUCTION, INC.  
Filing Number: 6137906

Application for Certificate of Authority

March 12, 1984

In testimony whereof, I have hereunto signed my name officially and caused to be impressed hereon the Seal of State at my office in Austin, Texas on November 09, 2021.



A handwritten signature in black ink, appearing to read "John B. Scott".

John B. Scott  
Secretary of State

Please see our authorization to conduct business in the State of Texas



# The State of Texas

SECRETARY OF STATE

AMENDED CERTIFICATE OF AUTHORITY  
OF

SUNDT CONSTRUCTION, INC.  
FORMERLY: SUNDT CORP.

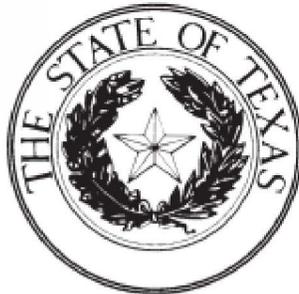
The undersigned, as Secretary of State of Texas, hereby certifies that an application of the above named entity for an Amended Certificate of Authority to transact business in this state, has been received in this office and is found to conform to law.

ACCORDINGLY the undersigned, as Secretary of State, and by virtue of the authority vested in the Secretary by law, hereby issues this Amended Certificate of Authority to transact business in this state under the name of

SUNDT CONSTRUCTION, INC.

and attaches hereto a copy of the Application for such Amended Certificate.

Dated: October 13, 1998  
Effective: October 13, 1998



 LCS

Alberto R. Gonzales  
Secretary of State

